

NORTH SAN JOSÉ URBAN DESIGN GUIDELINES



Table of Contents

| | |
|---|----|
| 1. INTRODUCTION | 5 |
| 2. URBAN DESIGN FRAMEWORK PLAN | 9 |
| 3. URBAN DESIGN GUIDELINES | 15 |
| Introduction and Guideline Organization | 16 |
| Guidelines for Site Planning | 21 |
| Block Size and Layout | 22 |
| Mix of Uses | 24 |
| Site Access and Circulation | 26 |
| Mid-Block Connections | 28 |
| Site Layout: | |
| Guidelines Overview | 32 |
| Building Orientation and Siting | 36 |
| Building Heights | 38 |
| Private Plazas in the Core Area | 40 |
| Private Pocket Parks and Plazas | 42 |
| Street Frontages | 44 |
| Retail | 52 |
| Transitions | 56 |
| Guidelines for Buildings | 61 |
| Building Massing | 62 |
| Building Articulation | 64 |
| Building Frontages and Entries | 66 |
| Building Design and Materials | 68 |

| | |
|--|-----|
| Guidelines for Parking | 79 |
| Surface Parking | 80 |
| Parking Structures | 82 |
| Bicycle Parking and Facilities | 84 |
| Guidelines for the Streetscape | 87 |
| Street Hierarchy and Typologies | 88 |
| Typical Street Sections | 90 |
| Paseos | 102 |
| Parkways | 104 |
| Streetscape Design | 108 |
| Guidelines for Parks, Trails, and Open Space | 113 |
| Parks, Trails, and Open Space Elements | 114 |
| Parks Design | 116 |
| Neighborhood Parks | 118 |
| Community Park | 120 |
| Central Urban Park/Plaza | 124 |
| Trails | 126 |
| Trail Design | 128 |
| Guidelines for Public Art | 135 |
| Public Art | 136 |
| 4. GLOSSARY, ACKNOWLEDGEMENTS, AND CREDITS | 139 |

Introduction

1

Introduction

Purpose and Intent of this Document

The North San José Guidelines are part of the Implementation Strategy for the North San José Area Development Policy. They will provide North San José–specific guidance to both private and public development and will implement the North San José Policy, which has created the opportunity for up to 26.7 million square feet of new office/industrial development, 32,000 residential units, and 1.7 million square feet of new commercial development.

North San José is in an area that has historically been reserved for industrial park development, typically one or two story buildings, with substantial setbacks from the street and surrounded by surface parking lots. These Guidelines will promote a more “urban” type of development than what has historically occurred in North San José, with respect to building design, orientation and placement.

Of particular importance is the visual and functional definition of the Core Area (North First Street Corridor) as a high-profile corporate center, which is an exciting and unique “place”, symbolic of the area’s leading role in the development and marketing of new technology and the premium location for technology industrial headquarters development in the Silicon Valley. Development in the Core Area will be more urban in character and more pedestrian and transit-oriented than the existing development in North San José.

Another aim of the Policy is to promote new residential development of an appropriate density to provide housing that is compatible with existing development, in walkable neighborhoods and close to jobs to reduce commute travel times, increase use of transit facilities and reduce traffic congestion.

In support of new residential and office development, the North San José Guidelines also aim to create successful retail districts in North San José, accessible by multiple modes of transportation at a pedestrian scale, to create places where people want to linger and spend time.

Interpretation and Application of this Document

The key economic development strategy for San Jose is to encourage driving industry companies to stay in San Jose and expand in San Jose. The North San Jose Area Development Policy was developed to allow and encourage that to happen in North San Jose.

The Guidelines represent a long-term vision and they are intended to facilitate development in a reasonable fashion that is financially viable and achieves the goals of the Policy. The Guidelines are not intended to be rigid or treated as a zoning code, and the Director of Planning, Building and Code Enforcement has substantial flexibility in applying the Guidelines. Capturing development opportunities for expansion of driving industry companies requires flexibility, creativity and timeliness to be competitive in the ever changing market for growing companies. Each development has unique challenges and the Director of Planning, Building and Code Enforcement is expected to exercise flexibility in solving those challenges to facilitate the development of sites to meet the needs of driving industry companies.

Periodic Review

The North San Jose Urban Design Guidelines shall be reviewed periodically to insure that they continue to work with tenant requirements and the real estate market.

Urban Design Framework Plan

2

Urban Design Framework Plan

Vision North San José

North San José is an important employment center for San José and an increasingly desirable place to live, due to its proximity to jobs, to natural and recreational amenities such as the Guadalupe River and Coyote Creek, and to Downtown San José. As North San José continues to develop as the main center for high-tech corporations in the Silicon Valley and as new residents relocate to the area, North San José will become significantly denser, transforming into an urban district that offers an attractive and balanced mix of uses, a walkable and bikable environment, and interconnected places for living, working, recreation, education, and shopping.

The Land Use Master Plan (*see Implementation Strategy*) illustrates the short- and mid-term vision for the area based on the goals stated in the North San José Development Policy and the work of the North San José Taskforce. The Urban Design Framework Plan presented here identifies long-term future opportunities based on the Land Use Master Plan, looking at a time span of approximately 30 years.

The key components of the Vision for North San José are:

- » **Promote Economic Activity** – Provide additional long-term development capacity to support the creation of up to 80,000 new jobs along North San José’s First Street corridor.
- » **Promote Livability** – Add new housing and retail development in close proximity to new jobs, amenities and transit infrastructure.
- » **Promote Long-Term Vitality** – Establish fair-share funding mechanisms for infrastructure improvements necessary to support new development.

This Framework Plan, in combination with the Urban Design Guidelines and the Implementation Strategy, will help shape and support the identity of North San José as a unique and attractive place that includes a pedestrian- and transit-oriented business center in its core, as well as livable and connected neighborhoods that support job growth.



New development should build on North San José’s identity as a leading center for technological innovation.



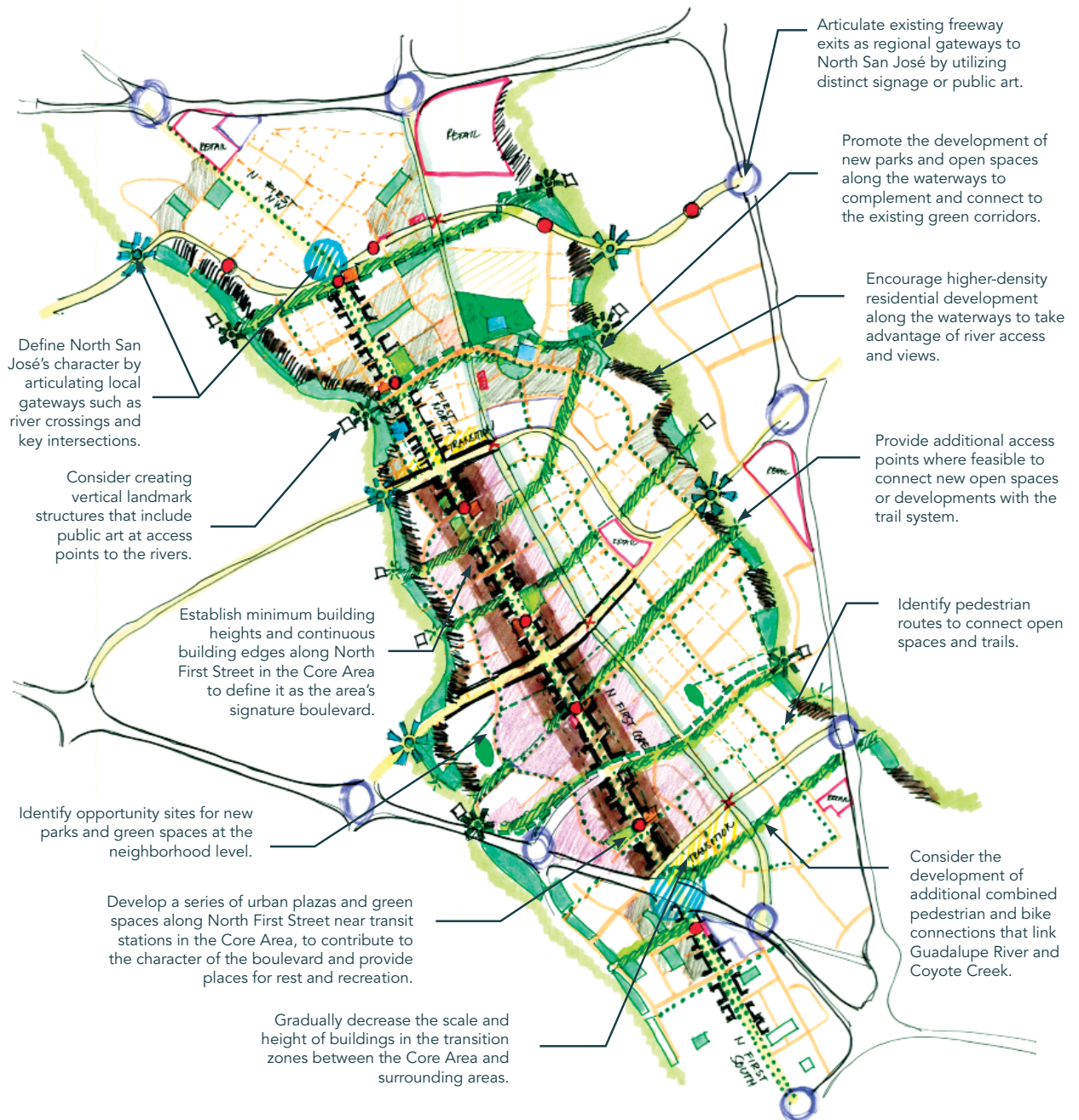
The Core Area will be the key focus area for industrial/commercial office development.



Residential neighborhoods will support and sustain increased job growth.

Long-Term Framework Plan

This conceptual diagram illustrates the long-term spatial vision for North San José. The key elements build on the North San José Development Policy and the North San José Guiding Principles (see separate reports). While some of these elements will be implemented in the short term (also see *Implementation Strategy*), others will be realized incrementally as development occurs over a longer period of time, roughly 15 to 30 years.



The key elements for the development of North José are: an increase in densities and in employment-based uses, particularly in the Core Area; improved overall walkability and bikability; the strengthening of east-west green connections linking the two waterways; improved waterway trail accessibility, clearer definition of the waterway edges; the strengthening of North First Street as a major north-south transit boulevard; the development of a network of parks and open spaces; and the provision of new public facilities.

Framework Plan Elements



Nodes and Gateways

Several important types of nodes and gateways define North San José. These include freeway crossings, street intersections at either end of the Core Area, and points where major thoroughfares cross the two waterways that bound North San José. Such locations are opportunities for public art, signage, and other features that can help make people aware that they are entering a distinct district within the city.



North-South Thoroughfares

North First Street and Zanker Road are the only north-south streets that traverse the entire length of North San José. Defining these streets with contrasting characters - North First Street as an urban, dense boulevard, and Zanker as a more lush and green street with bigger, landscaped setbacks - will give definition to the experience of traveling through North San José.



Green Network

One of the most special aspects of North San José is its proximity to not just one but two beautiful waterways, the Guadalupe River and Coyote Creek. These defining landscape features, already partially lined with popular trails, are an excellent opportunity upon which to build a distinct green network that links the waterway trails to other trails, open spaces, parks, and plazas, overlaying a network of nature within a urban environment.

Urban Design Guidelines

3

Urban Design Guidelines

Introduction

The key economic development strategy for San Jose is to encourage driving industry companies to stay in San Jose and expand in San Jose. The North San Jose Area Development Policy was developed to allow and encourage that to happen in North San Jose.

Interpretation and Application of These Guidelines

The Guidelines represent a long-term vision and are intended to facilitate development in a reasonable fashion that is financially viable. The Guidelines are not intended to be rigid or treated as a zoning code, and the Director of Planning, Building and Code Enforcement has substantial flexibility in applying the Guidelines. Capturing development opportunities for expansion of driving industry companies requires flexibility, creativity and timeliness to be competitive in the ever changing market for growing companies. Each development has unique challenges and the Director of Planning, Building and Code Enforcement is expected to exercise flexibility in solving those challenges to facilitate the development of sites to meet the needs of driving industry companies.

Given the nature of the Guidelines and that they are accepted by City Council and not adopted through ordinance or other means, additional flexibility is provided at the discretion of the Director of Planning, Building and Code Enforcement, to deviate from particular Guidelines as appropriate to meet the goals and intents of the Guidelines and the Policy. The intent of the document is to encourage creativity, in which case departure from Guidelines may be appropriate, particularly when it results in a higher quality design and project. In cases such as these, staff uses professional judgment to weigh the competing requirements of the development to design, the orientation of the site, the intent of the Guidelines and Policy, and the ability of the development to provide appropriate alternatives to promote a mutually acceptable solution so as not to create an impediment to development.

Relationship to Other Plans and Ordinances

The North San José Design Guidelines are intended to help implement the City's vision for North San José as established in the General Plan, North San José Area Development Policy, and Urban Design Framework Plan. These guidelines are applicable to the entire North San José Policy Area, including the Rincon South Specific Plan area.

The guidelines should be considered as a subordinate tool that provides specific details for how to implement the high-level policies contained in these documents. As such, in the event that the General Plan, or a Specific Plan, or other adopted City Ordinance specifically addresses an issue also addressed in these Guidelines so that there is a potential conflict, the General Plan, Specific Plan or Ordinance takes precedence. For example, the land use and design policies (setbacks, building height, etc.) in the Specific Plan for areas in Rincon South remain in effect and take precedence over the North San José Urban Design Guidelines in the event of contradictory direction. In general, however, these Guidelines have been developed to build upon these other policies and are expected to be consistent with them.

Phased Development

The Guidelines describe the design of developments at full build-out, and each phase of development should build towards that end state. A master plan for each site can demonstrate ultimate compliance with the Guidelines and show how the project could be phased.

Guideline Organization

The Guidelines are organized into six sections: Site Planning, Buildings, Parking, Streetscape, Open Space, and Public Art. Guidelines in the first three chapters apply to all areas within the private realm. Guidelines in the Streetscape and Open Space sections primarily refer to the public realm, but also include guidelines for areas that are privately owned or maintained and publicly accessible, such as paseos. Guidelines in the Public Art section concern public art in both the public and private realm.

Guiding Principles

Each guideline advances one or more of the following principles that are based on the North San José Development Policy and the work of the North San José Taskforce.



Walkability

Strive to make North San José districts and neighborhoods walkable.



Identity

Ensure that public development in North San José contributes to the strengthening of its identity.



Pedestrian-Friendly Environment

Create an urban environment that is visually attractive, pedestrian- and bike-friendly, and designed for the human scale.



Public Art

Integrate permanent or temporary public art to support North San José's identity and to provide attractive destinations for residents and visitors.



Connectivity

Encourage design and development within North San José that increases connectivity of uses, transportation and public facilities.



Sustainability

Integrate development strategies and design practices that are based on sustainable principles.



Mix of Uses

Foster a mix of uses, both horizontally and vertically, within developments and individual buildings.

Using the Guidelines

Each guideline is set up on a two-page spread with the same format. Below is an explanation of how to read the guideline.

1 **Site Access and Circulation**

2 Limit the number of curb-cuts along public streets by centralizing vehicular access. Proposed streets and pathways should connect to the public street system.

3 [Icons: speech bubble, car, person]

4 **Discussion**
Circulation on a site should be designed with publicly accessible streets that are connected to the area street network to encourage walking, and with clear and direct connections for pedestrians and bicycles. Vehicular movement across sidewalks (curb-cuts) should be minimized by locating driveways, parking courts, and parking garage entrances along the side or back of a building, along streets with less pedestrian traffic, thus enhancing the pedestrian environment and minimizing potential conflicts between pedestrians, bicyclists, and vehicles.

5 **Guidelines**

- Connect streets, paths and pathways to the public street network and to the open space system.
- Include street parking along proposed streets.
- Discourage dead-end streets and cul-de-sacs.
- Layout streets as a logical extension of the public street grid.
- Discourage curved or weaving streets, unless responding to the topography or other conditions.
- Reduce curb-cuts by centralizing vehicular access to the site. Curb-cuts are not allowed along North First Street except for shared street access or passes that are part of the larger street network.
- Locate vehicular circulation, including parking, service and loading zones, on the side or the rear of a building, away from the main building front. Central vehicular entrances through integration in the building facade (also see Parking Guidelines).
- Provide as many pedestrian and bicycle access points from public streets as possible. At the very least, pedestrian and bicycles should be able to directly access the building from the street at each building entrance.
- Create straight paths for pedestrians that respond to real pedestrian needs (e.g. from transit station exit to building, street corner to building, building to building, etc.) rather than meandering paths that serve as decorative landscape features.
- Parking garages should not front primary streets or should be wrapped with habitable spaces or retail (also see Parking Guidelines).
- Parking garages are not allowed along North Fort Street and should be located within a minimum of 200 feet from North First Street unless they include habitable spaces facing the street edge (also see Parking Guidelines).

6 [Photograph 1] Locate drop-off and curbs on the side of building and working from with sidewalk connections.

6 [Photograph 2] Locate parking garage entrances on the side of building and integrate them into the building facade.

118 North San José Urban Plan 119 North San José Urban Plan

- ① The guideline's title.
- ② One sentence that states the main intent of the guideline.
- ③ Icons of the principles that are advanced to this guideline.
- ④ A discussion of the ideas and rationale behind the guideline.
- ⑤ The specific guideline text.
- ⑥ Diagrams and images that explain and show examples of the guideline in action.



Guidelines for Site Planning

Block Size and Layout

Mix of Uses

Site Access and Circulation

Mid-Block Connections

Site Layout: Guidelines Overview

Site Layout: Building Orientation and Siting

Site Layout: Building Heights

Site Layout: Private Plazas in the Core Area

Site Layout: Private Pocket Parks and Plazas

Street Frontages

Retail

Transitions

Block Size and Layout

Connect the block layout to the larger urban fabric to create a walkable environment. Minimize block sizes as much as possible.



North San José: 1,000 feet by 2,200 feet



Downtown San José: 275 feet by 550 feet



Downtown Portland: 200 feet by 200 feet

These aerial images, all at the same scale, show how North San José's street grid is much more spread out than that of walkable areas such as downtown San José and downtown Portland, Oregon.

Discussion

Increased pedestrian activity is key to the development of North San José as a vibrant, sustainable district within the city. Pedestrian activity also helps generate higher rates of transit ridership by encouraging the use of alternative transportation options. However, large blocks with lengths of a quarter-mile or more can discourage pedestrians from walking, especially where there are few connections and little variety in the passing landscape. Shorter block lengths make the apparent distance between any two points seem shorter to pedestrians. Walking can be promoted by establishing a gridded system of streets with shorter block lengths in North San José.

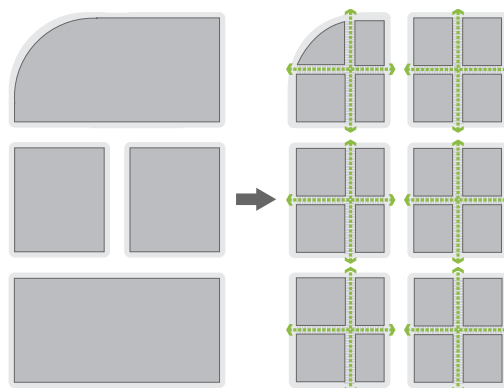
Four Ways to Divide Large Blocks

Grid Streets are mandatory in the Core Area, except for single-user campuses, but their alignment may vary when indicated on the North San Jose Grid System Master Plan. The street may be private for a single user campus but should match public street design and be encouraged to be open to the public.

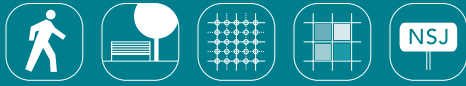
Other Streets used to divide large blocks shall meet the requirements of the Guidelines for the Streetscape chapter. The street may be private for a single user campus but should match public street design and be encouraged to be open to the public.

Paseos may be used to divide residential blocks and shall meet the requirements of the Guidelines for the Streetscape: Street Hierarchy and Typologies: Paseos. They shall be open to the public.

Mid-Block Connections or Pathways (the terms are used interchangeably) may be used to divide non-residential blocks and shall meet the requirements of the Guidelines for Site Planning: Mid-Block Connections. Access may be limited to tenants and visitors, but the pathways should provide convenient and direct access from the site to transit and amenities.



Subdivide larger blocks through streets, paseos or pathways.



Walkability
Pedestrian-Friendly Environment
Connectivity

Mix of Uses
Identity

Guidelines

- » Encourage the subdivision of large super-blocks into smaller blocks with additional streets, paseos, and pathways for improved connectivity and orientation.
- » Align and connect new streets, paseos, and pathways with existing and planned streets.
- » All blocks should have either public or publicly-accessible streets or paseos surrounding them.
- » Develop block layouts that allow for a denser and more fine-grained urban fabric and allow for the integration of residential and commercial development as well as open spaces.
- » When laying out new streets, consider natural site features such as large trees, streams, or topographic changes.
- » These guidelines do not apply to portions of the site boundary where connections cannot be made because of physical obstacles, such as existing buildings, water bodies and wetlands, railroad and utility rights-of-way, limited access roads, parks and dedicated open space and extreme topography.

Core Area

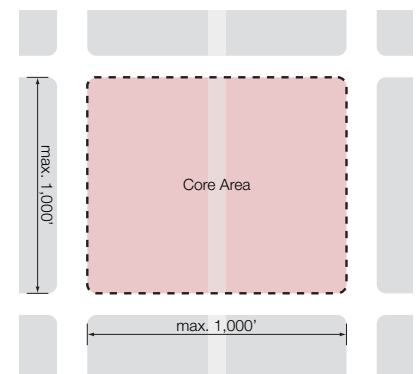
- » The maximum block size should not exceed approximately 1,000 feet on either side to provide a high level of flexibility for different uses and site layout needs.
- » Blocks with lengths ranging between 600 and 1,000 feet on any one side should provide mid-block connections in the form of paseos or pathways to increase walkability.
- » The additional break-up of blocks is encouraged to achieve ideal pedestrian-friendly blocks, with lengths typically ranging between 300 and 400 feet on both sides.

Industrial Park and Office Areas

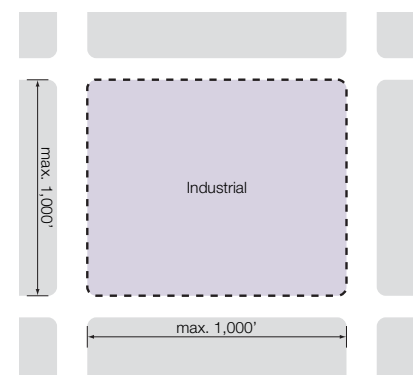
- » The maximum block size should not exceed approximately 1,000 feet on either side to provide a high level of flexibility for different uses and site layout needs.

Residential Areas

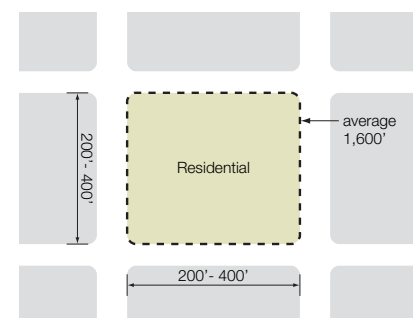
- » In residential areas, blocks should have an average perimeter length of 1,600 feet, defined by a street or paseo that meets the streetscape standards (see *Guidelines for the Streetscape*) to encourage walking.
- » Encourage small blocks in all areas where feasible. Generally, walkable block lengths range from 200 to 400 feet.



Core Area blocks should have a maximum width of 1,000 feet; if any side is longer than 600 feet, it should include a mid-block connection.



Industrial and commercial blocks should have a maximum width of 1,000 feet.



Residential blocks should have an average perimeter length of 1,600 feet.

Mix of Uses

Encourage the vertical and horizontal mixing of uses, particularly through ground-floor retail or the combination of residential and office uses where allowed.

Discussion

A mix of uses in buildings and sites creates a finer-grained pedestrian-level fabric, engaging the senses and adding variety to the human experience. The environment benefits, too. By bringing important destinations close together, a mix of uses also increases convenience for pedestrians, particularly when such sites are co-located with or near their home or office. When people can complete several functions at one location, they can reduce overall trips, and therefore reduce congestion and pollution. Mixed-use projects that are inviting destinations and function well for the user and the environment, distinguish North San José as a desirable place to work, live and play.



Buildings with a mix of uses, like this one with office and residential uses above a grocery store, can help reduce auto trips and generate more interesting buildings.



Ground-floor retail should be well-integrated into the design of mixed-use residential buildings.



Pedestrian-Friendly Environment
Mix of Uses

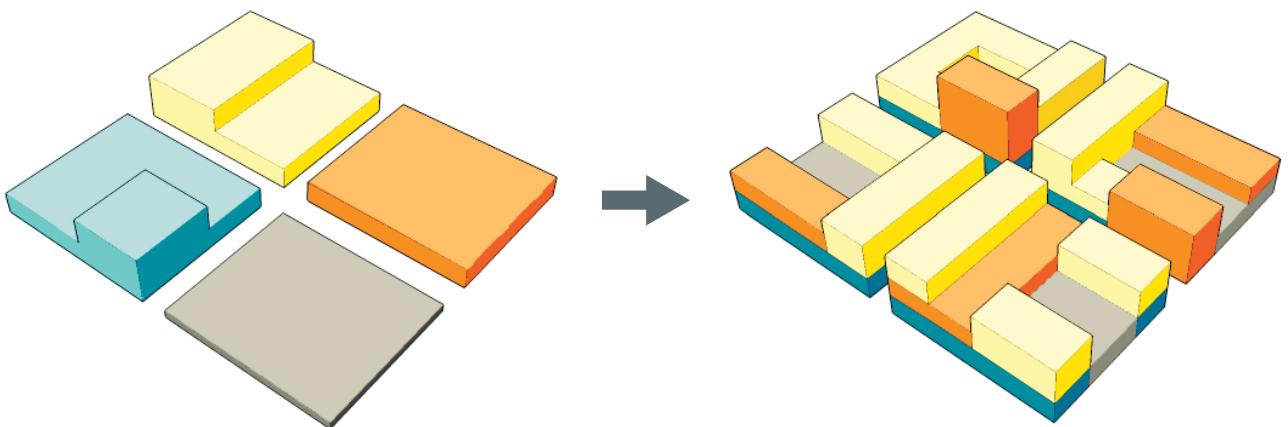
Identity
Sustainability

Guidelines

- » Foster mixed-use developments that combine important functions and attractive design to create places that work well for people and encourage environmentally beneficial behaviors.
- » Consider amenity retail in mixed-use development where retail site criteria are met (*see the Retail guideline in this section for the definition of amenity retail*).
- » Encourage mixed-use buildings that combine public and private uses, for example libraries or post offices that are integrated into mixed-use buildings.
 - » Use secondary frontages or second-floor spaces for non-retail uses rather than displacing retail from space that best meets the retail site criteria.
 - » Encourage circulation among these uses to increase functionality and customer patronage.
- » Encourage shared use of public facilities, especially in neighborhood mixed-use developments. Examples include schools and parks that share sports fields.
- » Encourage mixed-use buildings that include structured parking at the back of buildings and within the block. Provide only 10% of retail parking at the curb or adjacent to retail as “teaser” parking (*also see Guidelines for Parking*).



Public uses, like this library, can be located on the ground floor of mixed-use buildings.



Instead of a horizontal separation of uses that creates single-use blocks....

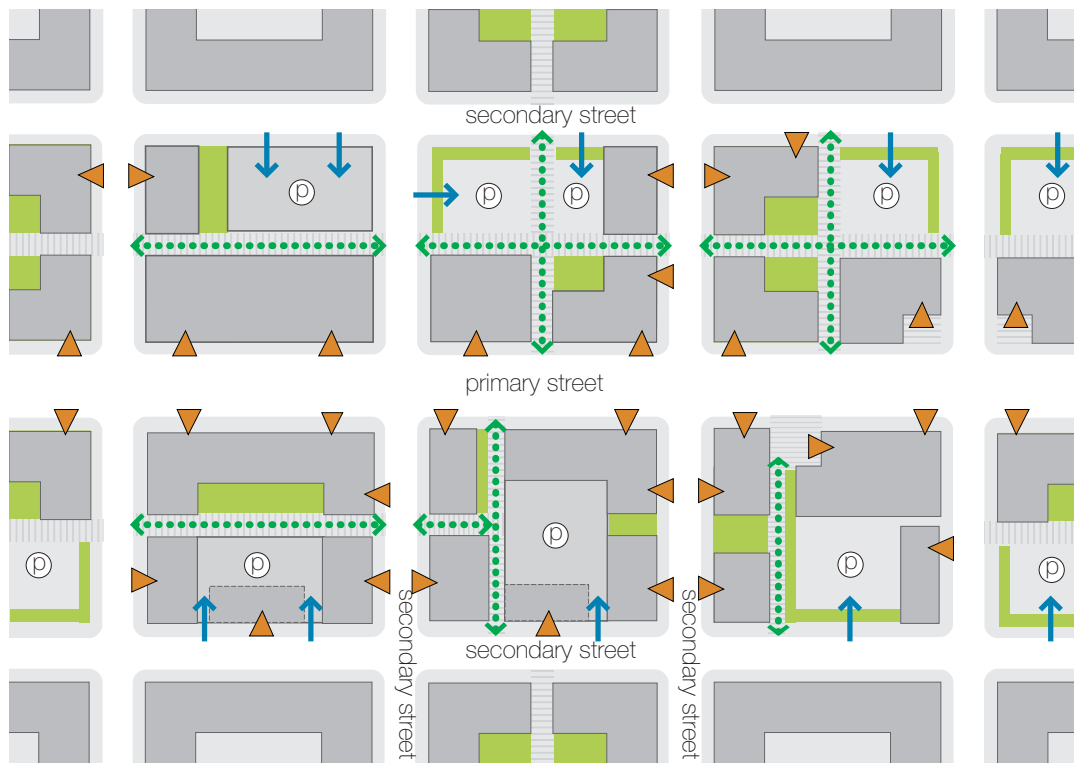
.... encourage vertical and horizontal mixing of uses to create a finer-grained, pedestrian-friendly urban fabric.

Site Access and Circulation

Limit the number of curb-cuts along public streets by centralizing vehicular access. Connect proposed streets and pathways to the public street system.

Discussion

Circulation on a site should be designed with publicly-accessible streets that are connected to the area street network encouraging walking, and providing clear and direct connections for pedestrians and bicyclists. Vehicular movement across sidewalks (curb cuts) should be minimized by locating driveways, parking courts, and parking garage entrances along the side or back of a building, along streets with less pedestrian traffic, thus enhancing the pedestrian environment and minimizing potential conflicts between pedestrians, bicyclists, and vehicles.



This diagram shows how site and building access can be oriented for several different block configurations.

- main building entrance
- vehicular access
- pedestrian connections/limited vehicular access
- surface or structured parking



Pedestrian-Friendly Environment
Identity
Sustainability

Guidelines

- » Connect streets, paseos and pathways to the larger public street network and to the open space system.
- » Include street parking along proposed streets.
- » Discourage dead-end streets and cul-de-sacs.
- » Lay out streets as a logical extension of the public street grid.
- » Discourage curved or weaving streets, unless responding to site topography or other conditions.
- » New curb-cuts should be minimized, particularly along North First Street.
- » Where feasible, encourage shared and consolidated site access, and use new streets or driveways that resemble publicly-accessible streets.
- » Locate vehicular circulation, including parking, service, and loading zones, on the side or the rear of a building, away from the main building front.
- » Conceal vehicular entrances by integrating them into the building façade (also see *Guidelines for Parking: Structured Parking*).
- » Provide as many pedestrian and bicycle access points from public streets as possible. Pedestrian and bicyclists should be able to directly access the building from the street at each building entrance.
- » Create straight pedestrian paths that respond to real pedestrian needs (e.g. from transit station exit to building or street corner to building) rather than meandering paths that serve as decorative landscape features.
- » Parking garages should not face onto North First Street.



Locate driveways and courts on the side of buildings and combine them with mid-block connections.



Locate parking garage entrances on the side of buildings and integrate them into the building façade.

Mid-Block Connections

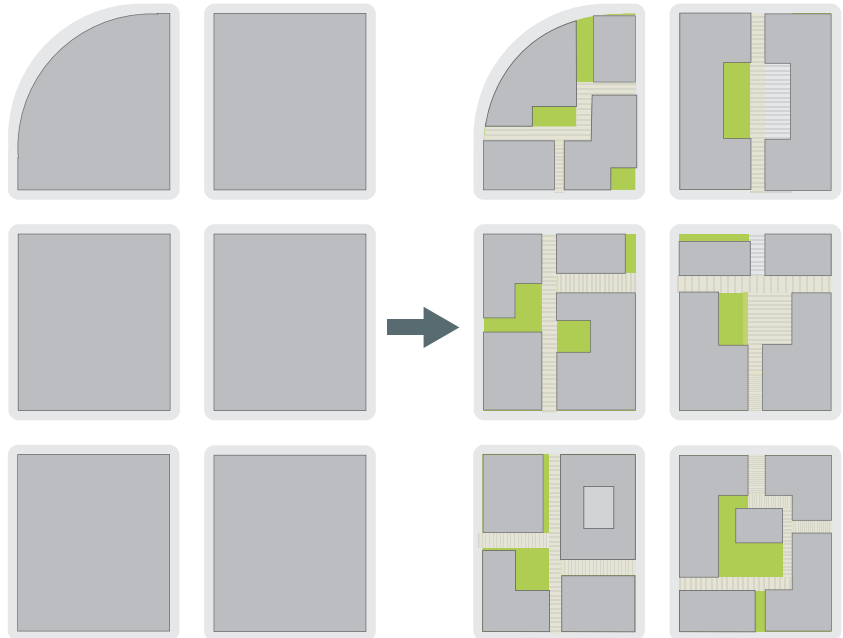
Encourage additional mid-block connections wherever feasible to complement the public and private street network and to increase overall walkability in North San José.



Mid-block connections can become a shared asset for residents, employees, and the general public.

Discussion

Small pedestrian paths provide shortcuts for pedestrians and bicyclists, increasing visibility and accessibility between different areas within North San José and thus increasing activity levels as well. On private sites, pedestrian paths that are separated and protected from vehicular traffic and parking can offer relief from an auto-oriented landscape. Furthermore, by connecting employees, visitors, and residents to open space, they can become a shared asset enjoyed by all.



Mid-block connections contribute to a finer-grained urban fabric and improve overall connectivity for pedestrians and cyclists.

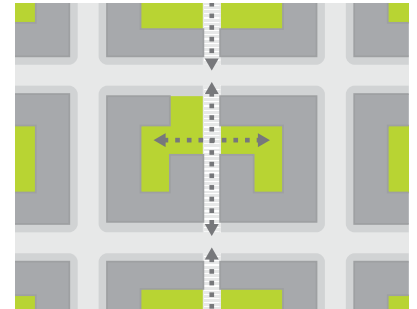


Walkability
Pedestrian-Friendly Environment

Connectivity
Identity

Guidelines

- » Mid-Block Connections or Pathways (the terms are used interchangeably) may be used to divide non-residential blocks and shall satisfy these Guidelines. Access may be limited to tenants and visitors, but the pathways should provide convenient and direct access from the site to transit and amenities.
- » Encourage publicly-accessible pedestrian paths through larger, single-use developments, such as office campuses or residential complexes, to provide a walkable and bikable environment for residents, employees, and visitors.
- » Align internal paths with pathways and mid-block connections on adjacent sites to allow for movement through multiple blocks.
- » Ensure that access points to mid-block connections are visible from public and publicly-accessible streets.
- » Cross-site connections should be planned as shared bicycle and pedestrian paths.
- » The width of mid-block connections should range between 20 and 40 feet.
- » A designated pedestrian path should have a minimum width of 10 feet.
- » Include building entrances on mid-block pathways as well as on streets.
- » Variations in materials, street furniture and tree and plant species are encouraged if they add to the character and quality of the streetscape.
- » Use high-quality and sustainable materials for pavement, street furniture, light and fences.
- » Develop creative solutions to address security while maintaining walkability; for example, provide access during daytime hours only or limit access to tenants and residents.
- » Mid-block connections should be at grade. If a grade change is necessary, for example on top of a parking podium, the change should not be greater than four feet to ensure the visibility and accessibility of the path.
- » Include pedestrian-scale public art in mid-block connections through incorporation into amenities, building enhancements, and wayfinding, and through standalone artworks. Pedestrian thoroughfares provide important opportunities for narrative or sequential engagement.



Connect mid-block paths to interior plazas and courtyards.



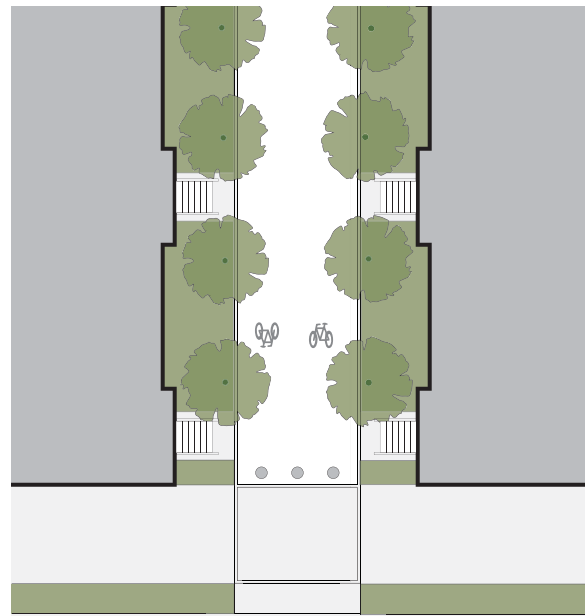
Mid-block pathways that connect to public streets with clearly visible access points can increase walkability.



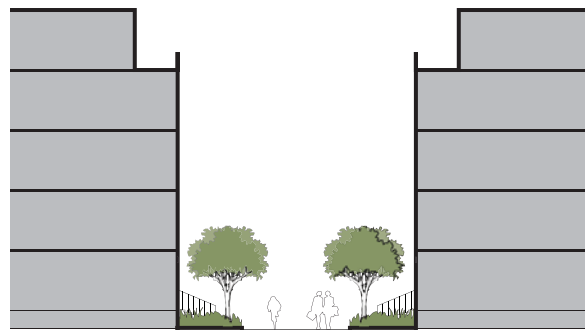
Limited grade changes ensure visibility and public character. Ramps and stairs per the ADA should be integrated in the design.

Mid-Block Connections

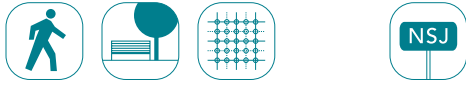
1a Pathway (20 to 40 feet wide)



yard 10' typ. | shared bike/pedestrian path 20' typ. | yard 10' typ.
face-of-building to face-of-building 20' - 40'



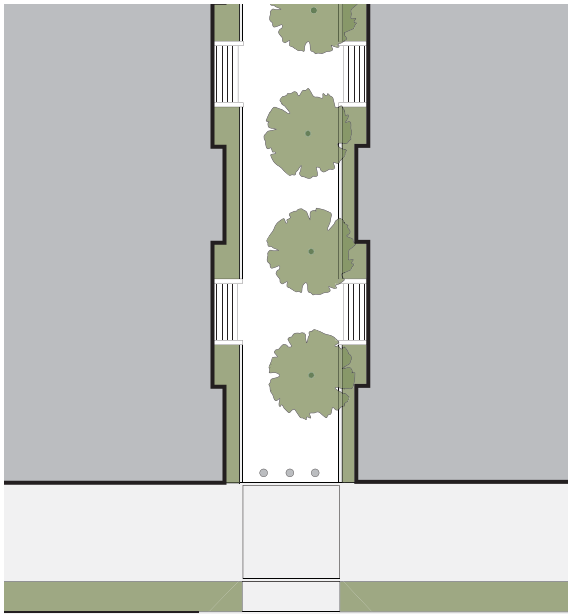
yard 10' typ. | shared bike/pedestrian path 20' typ. | yard 10' typ.
face-of-building to face-of-building 20' - 40'



Walkability
Pedestrian-Friendly Environment

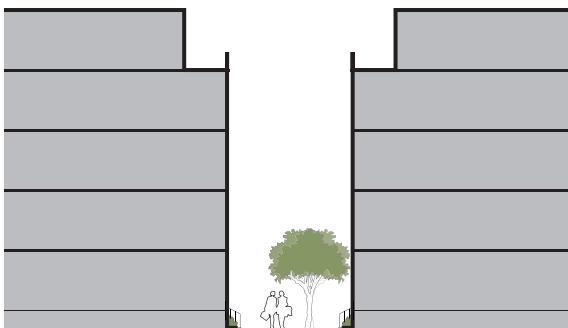
Connectivity
Identity

1b Pathway (20 feet wide or less)



shared bike/
pedestrian path
varies

face-of-building
to face-of-building
20' maximum



shared bike/
pedestrian path
varies

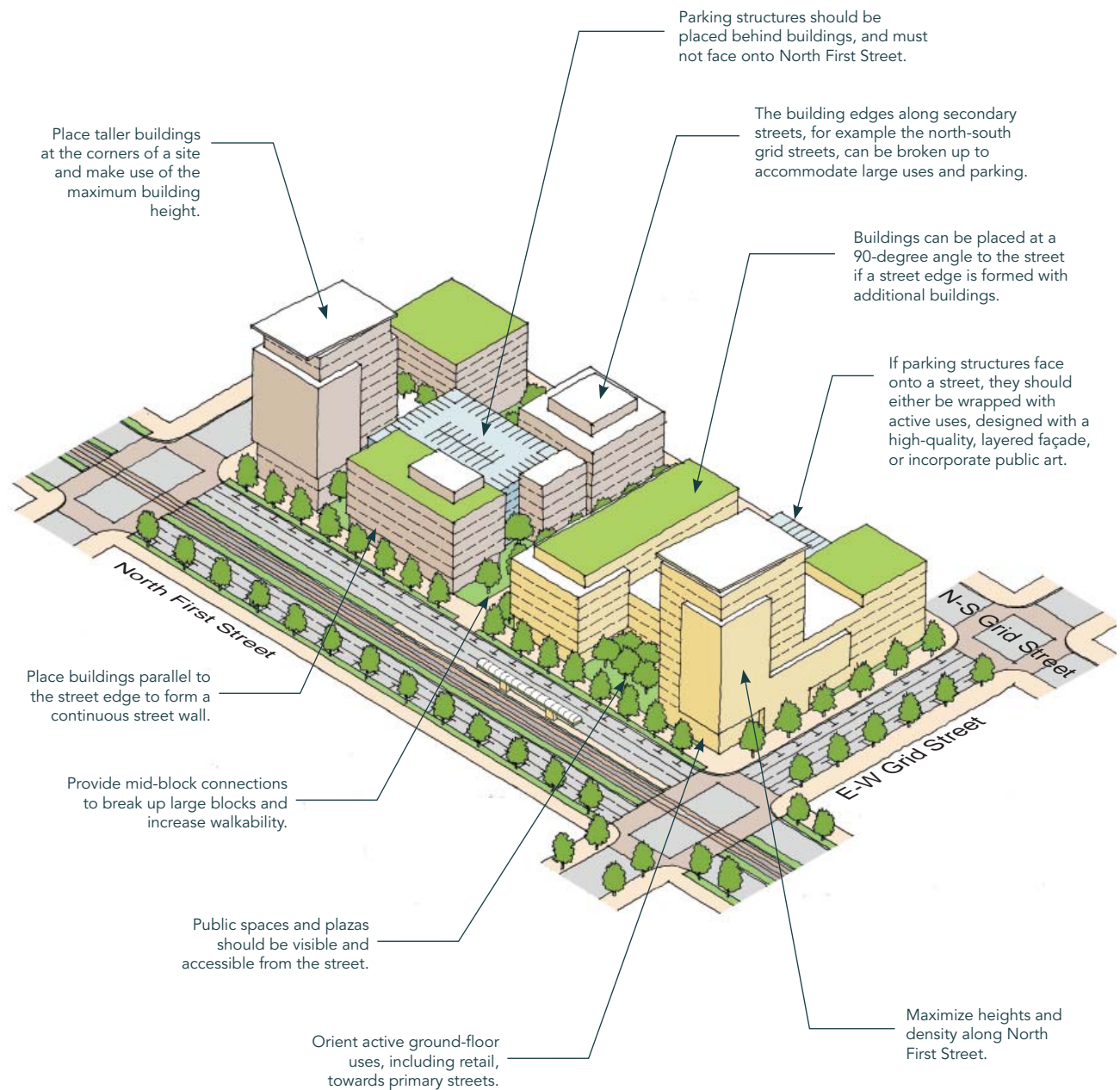
face-of-building
to face-of-building
20' maximum



Mid-block paths up to 20 feet wide can add another layer of walkability and bikability by creating more intimate connections.

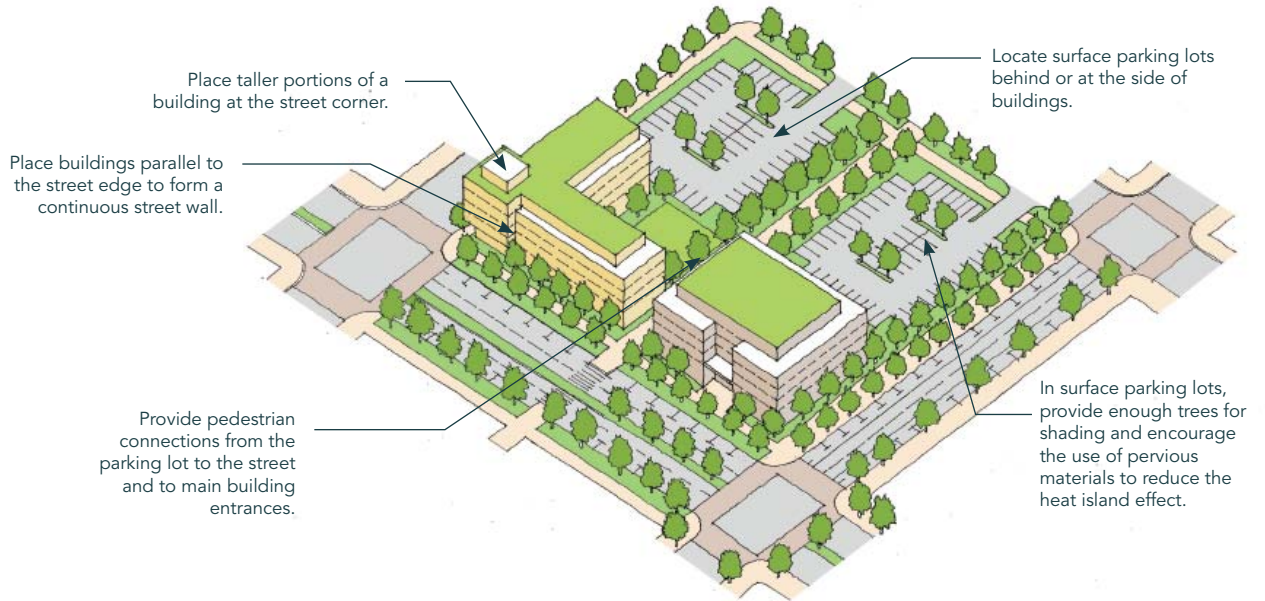
Site Layout: Guidelines Overview

Core Area/Mixed-Use

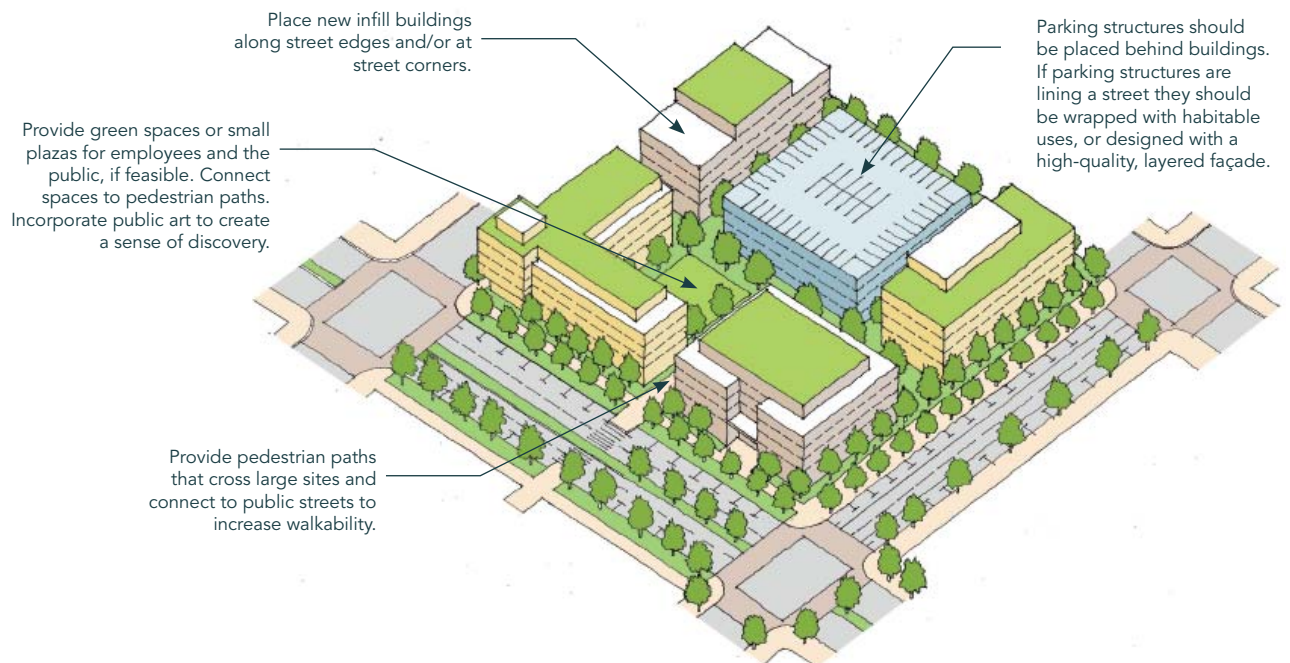


Industrial Park Areas

Phase 1: Layout with Surface Parking



Phase 2: Layout with Structured Parking



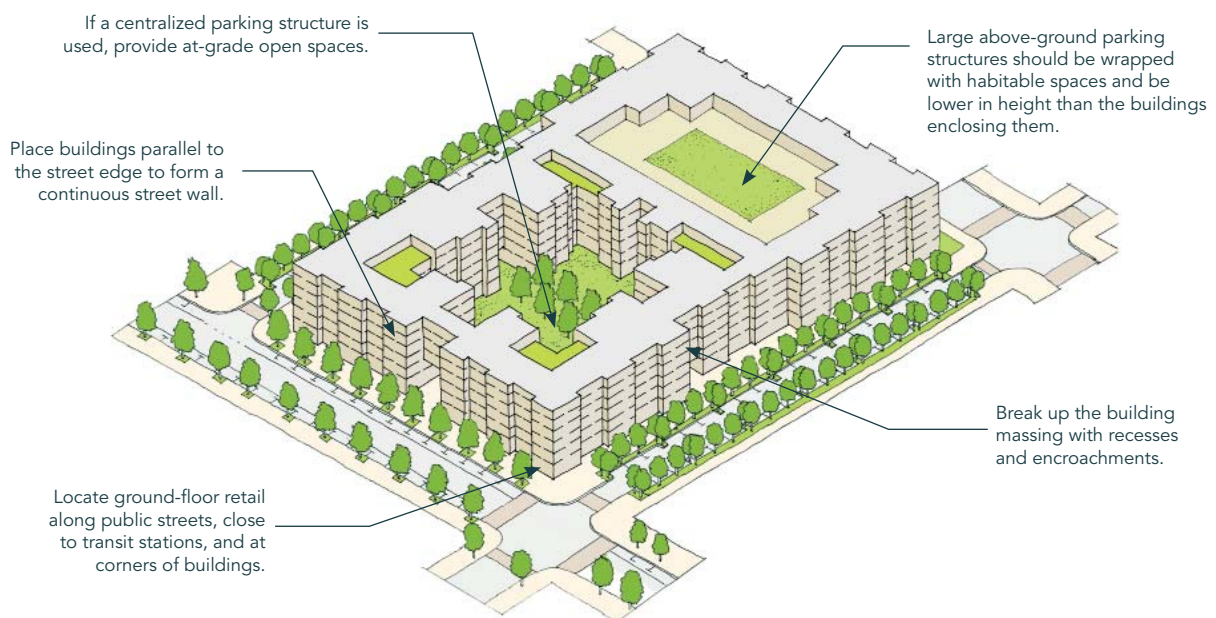
Site Layout: Guidelines Overview

Residential/Mixed-Use

Residential Block with Mid-Block Connection

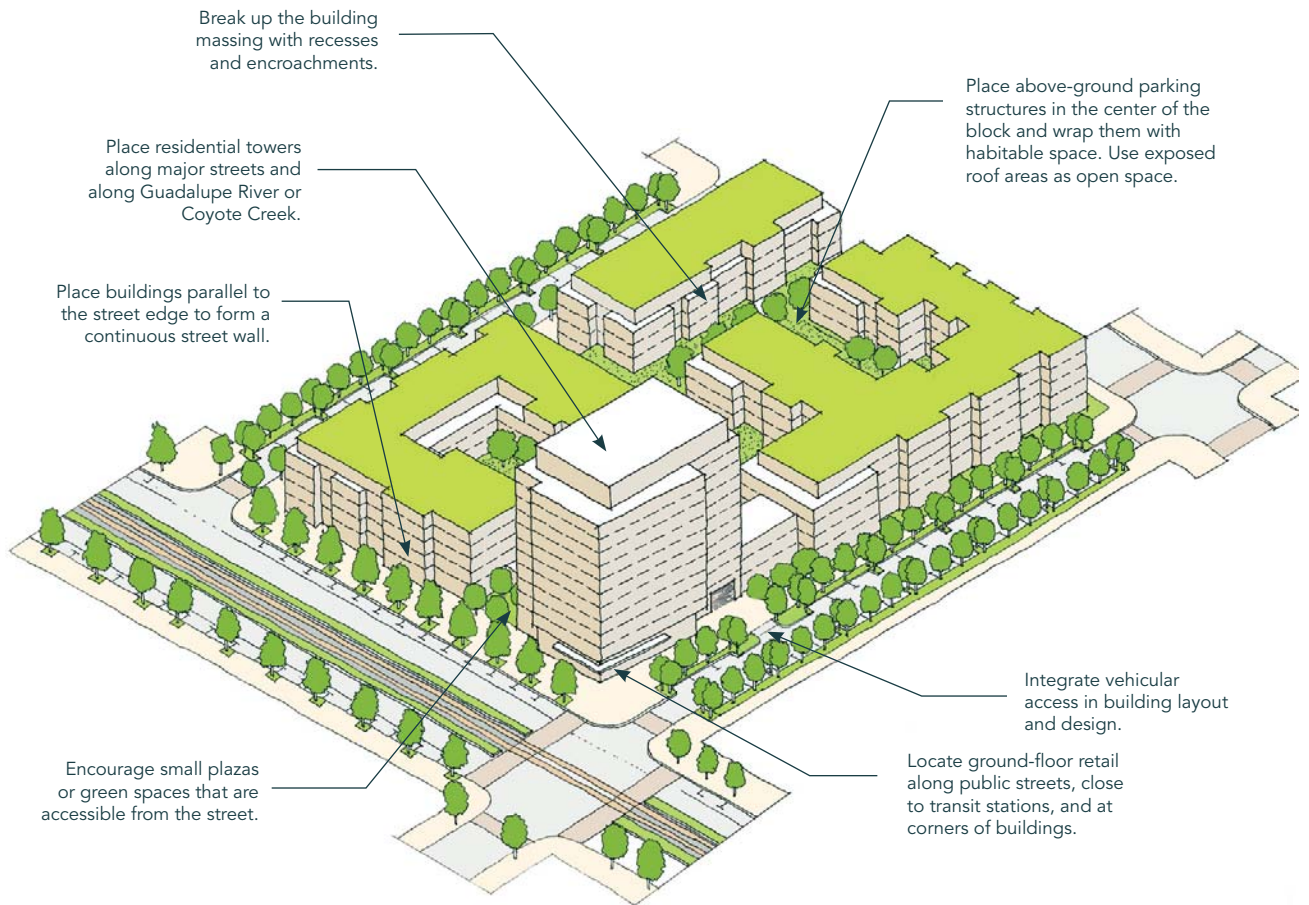


Residential Block with consolidated Parking Structure and at-grade Open Space



Residential/Mixed-Use

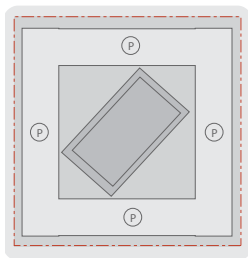
Residential Block with Tower (maximum height of 120 to 150 feet)



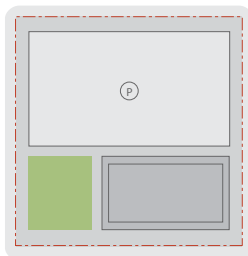
Site Layout: Building Orientation and Siting

The layout of a site should respect natural conditions and assets, fit into the existing and planned urban fabric, and optimize access and visibility.

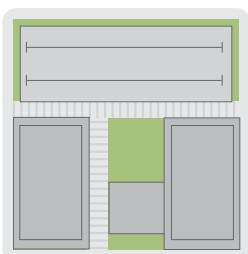
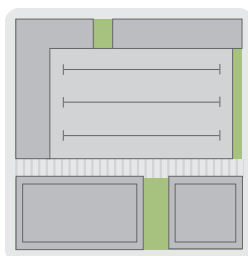
Office/Industrial Site Layout



Discouraged: Angled building orientation surrounded by surface parking lots.



Encouraged: Building placed parallel to the street with surface parking lot in back.



Encouraged: Building orientation parallel or perpendicular to the street with “wrapped” or screened structured parking garage placed behind the building.

Discussion

Site layouts that ignore local site conditions such as sun orientation, topography, trees, streets, and waterways can contribute to a lack of sense of place and disorientation. Conversely, building arrangements that are connected to the existing urban fabric and that respond to local site conditions reinforce and strengthen the identity of North San José as a vibrant, sustainable neighborhood. Buildings oriented to the street stress the importance of the public realm, create a more continuous urban experience, and thus add to an area’s identity. Vehicular circulation and parking should become an integral but not dominant part in the urban environment. While the guidelines for commercial and office development apply to all areas including the Industrial Park Area, they are particularly important for future development in the Core Area.



These images show a before-and-after scenario of good site infill, where existing buildings are kept, new street-facing buildings are built on existing parking lots, and surface parking is replaced with parking structures interior to the block.



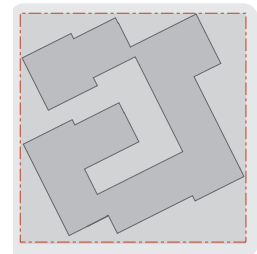
Pedestrian-Friendly Environment
Connectivity

Identity
Sustainability

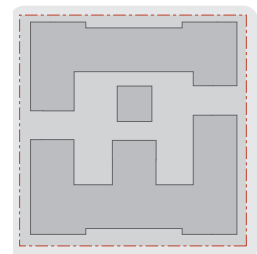
Guidelines

- » Buildings should be oriented as follows:
 - » Buildings should be oriented parallel to existing streets and along the edges of a site to create a tight urban fabric.
 - » A perpendicular orientation should only be considered for taller buildings, or if the buildings form a street-accessible park or plaza.
 - » If taller buildings are oriented perpendicular to the street, a shorter building portion should be placed parallel to the street to form a continuous street wall.
 - » Also see *Street Frontages* and *Guidelines for Buildings: Building Massing* for more detailed guidelines.
- » Avoid placing buildings at an angle to the street or with large convex forms facing the street.
- » Maximize a building's active spaces along its public street perimeter.
- » Use sustainable design practices early on in the site planning process in accordance with City's policies for green design.
- » Respond to the existing and planned context:
 - » Integrate and connect to local and on-site natural assets such as streams, large trees, or topography.
 - » Connect to built assets such as pedestrian paths, parks, trails, and public buildings that are on or near the site.

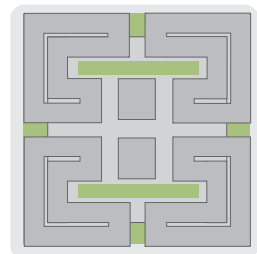
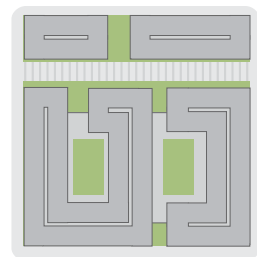
Residential/Mixed-Use Site Layout



Discouraged: Residential buildings that have an angled orientation along a street.



Encouraged: Residential buildings that form a clear edge parallel to the street.



Encouraged: Smaller building blocks that allow for courtyards, pedestrian paths and alleys, and "wrapped" parking structures.

Site Layout: Building Heights

Encourage building to the North San José Policy height limits to create an urban district, particularly in the Core Area. Concentrate taller buildings along North First Street.

Discussion

A crucial element of any vibrant, active urban district is people – there must be a critical mass of residents, area employees, and visitors to activate a place and give it life. This can only happen if building heights allow for the necessary density of people. In particular, tall buildings should be concentrated around light rail stations to encourage transit usage, and located adjacent to the rivers so that residents can enjoy this natural asset on a regular basis.



Place taller buildings along waterways to take advantage of river access and views.



Place taller buildings at intersections to create a building sequence along the street wall.

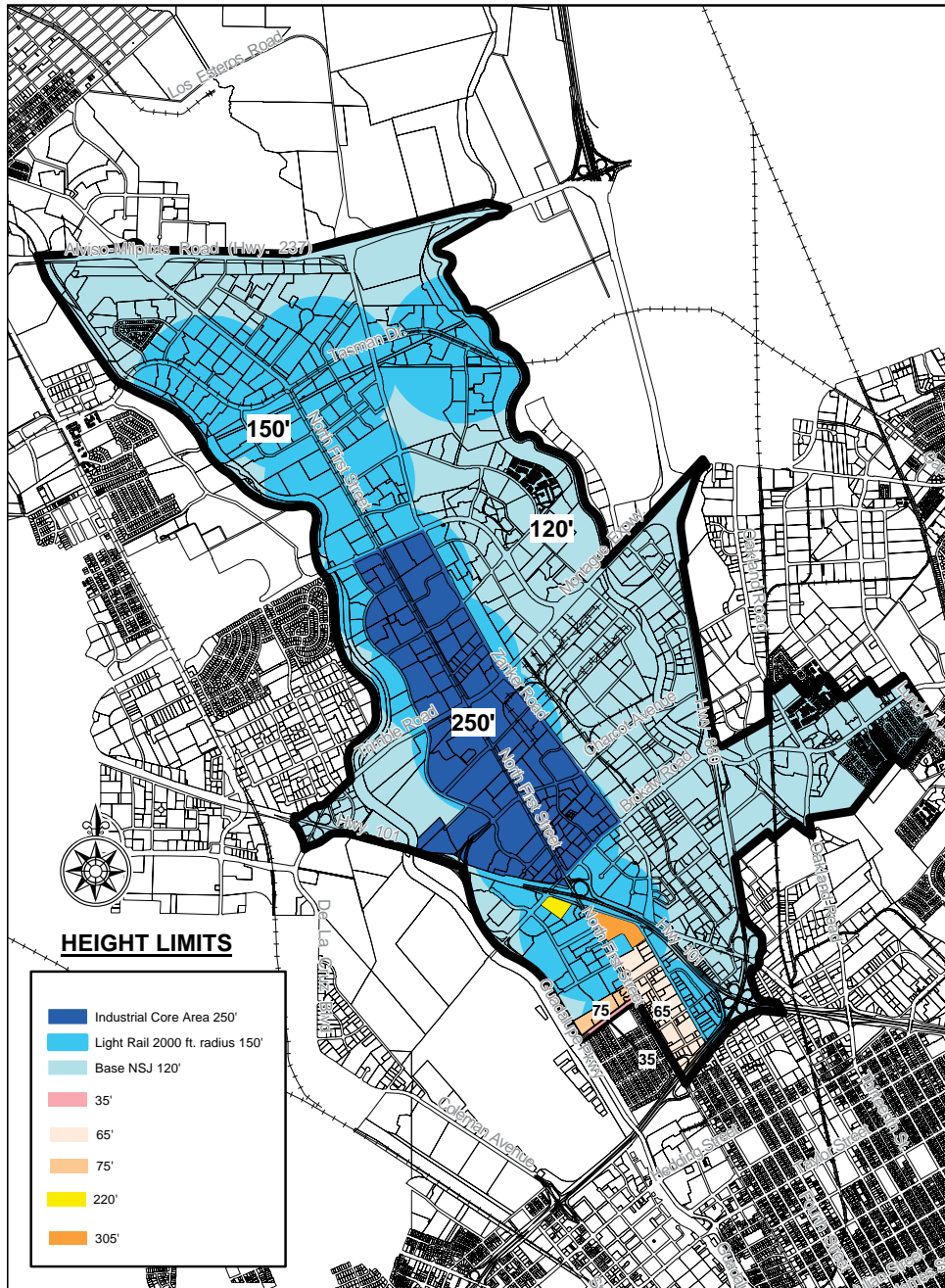
Guidelines

- » The tallest buildings in North San José should be located along North First Street.
- » New buildings facing North First Street in the Core Area should be at least 4 stories in height and preferably taller.
- » Place accent towers of up to 250 feet in height at major intersections.
- » Taller buildings on North First Street should be concentrated around light rail stations to encourage transit ridership.
- » In addition to North First Street, taller accent buildings are encouraged along the waterways to take advantage of river access and views.
- » In areas zoned as Industrial Park, the typical building height is 60 feet and the maximum building height is 120 feet.
- » In areas zoned as Light Industrial, the maximum building height is 120 feet.
- » In transition zones between the Core Area and surrounding areas, building heights should incrementally step down.
- » Individual projects may be subject to Federal Aviation Administration (FAA) review.



Walkability
Connectivity
Mix of Uses

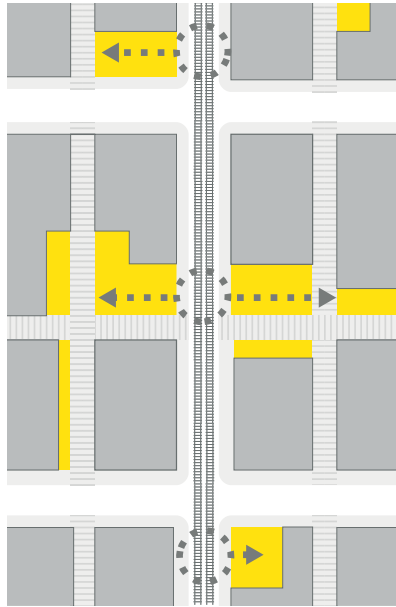
Identity
Sustainability



Shown here are height limits as per the City's General Plan.

Site Layout: Private Plazas in the Core Area

Develop a private network of publicly-accessible plazas in the Core Area.



Create a network of plazas visible from transit and public corridors.

Discussion

As the Core Area increases in height and density with new development, and the character and scale of North First Street becomes more urban, a network of small- to medium-sized plazas can offer spaces where workers, residents and visitors can enjoy the outdoors, and San José's mild climate. By distributing these plazas evenly throughout the Core Area, connecting them physically and visually to the North First Street corridor, and lining them with active ground-floor uses, these urban open spaces will contribute to the development of a lively and active commercial center.

Guidelines

- » Plazas should be urban in character, open to the public, and roughly one-half to one and one-half acres in size. Plazas should be designed in proportion to adjacent buildings.
- » Plazas should be placed in locations that are close to and visible from transit stations along North First Street.
- » Orient plazas for high visibility to promote usage by non-residents, thus stimulating activity throughout the day and into the evening.
- » Plazas should be well-integrated into the site or building layout.
- » Plazas should be connected to public and publicly-accessible pathways within the Core Area to improve walkability.
- » Plazas should include amenities for daily use by employees and visitors such as lunchtime seating in both sunny and shaded areas.
- » Public art, or places for changing artwork, should be incorporated. Additionally, consider including artists on design teams.
- » Design plazas to accommodate small- to medium-scale events such as lunchtime concerts, performances, promotional events, and exhibitions.
- » Encourage pedestrian-friendly and interactive uses, such as retail, cafés and restaurants, for ground-floor uses in buildings surrounding the plaza.



Walkability
Pedestrian-Friendly Environment
Connectivity

Identity
Public Art
Sustainability



Provide high quality, modern landscaping that support a variety of active and passive areas.



Provide plazas that are visible and accessible from the street to attract employees and visitors alike.

Guidelines (continued)

- » Encourage these surrounding uses to spill out into and engage the plaza through features such as restaurant/café seating and outdoor displays of retail merchandise.
- » Plazas should include areas for both active and passive use as well as daytime and nighttime use.
- » Utilize durable, high-quality materials for pavement areas, seating areas, and signage. Use sustainable materials when possible.
- » Landscape design of the highest quality is encouraged. Include abundant shade trees where needed.
- » Encourage the placement of smaller-scale plazas along secondary streets that can form a network along with the plazas along the North First Street corridor.
- » Where feasible, provide connections between the plazas.

Site Layout: Private Pocket Parks and Plazas

Create new publicly-accessible, private parks and plazas throughout North San José to provide intimate, local open spaces in developed areas.



Break up dense development with a network of small, informal-use open spaces.

Discussion

As the smallest component of the open space network, pocket parks and smaller plazas are intended to be used as intimate, local open spaces throughout North San José. They may be short stretches of streets with wider-than-normal sidewalks or small plazas at corners where buildings are set back locally from the street, or they may be nested within a block or building cluster. These spaces are intended to be primarily passive, with areas for sitting and strolling. Small-scale open spaces like these can foster casual social interaction and a sense of community, helping to define the public realm and urban culture.



Plazas can be as simple as a small setback from the street edge that provides a sunny spot for office workers at lunchtime.



Walkability
Pedestrian-Friendly Environment
Connectivity

Identity
Public Art

Guidelines

- » Pocket parks and plazas can range from a few hundred square feet up to one-quarter or one-half acre in size.
- » Utilize allowed setbacks to create publicly-accessible widened sidewalks, waiting places at bus stops, or small plazas on corners.
- » Pocket parks may also be nested within a building cluster, provided that they are connected to the pedestrian network.
- » Consider the inclusion of pocket parks during initial site planning phases in order to avoid creating “leftover” spaces.
- » Develop creative spaces by incorporating public art into space design, and by creating amenities and standalone art projects.
- » Encourage active ground-floor uses along the edges of pocket parks and plazas.
- » Utilize durable, high-quality materials for pavement areas, seating areas, and signage. Use sustainable materials when possible.
- » Landscape design of the highest quality is encouraged. Include shade trees as needed, and when recycled water for irrigation is not available, use native and drought-tolerant plants.
- » Encourage public access; if necessary for security reasons, allow public access only during daytime hours.



Small plazas and green spaces combined with a neighborhood center can be part of a mid-block pedestrian path or trail.



Encourage commercial or retail ground-floor uses that activate adjacent plazas.

Street Frontages

Orient new buildings to public streets to contribute to a pedestrian-friendly streetscape. Use setbacks and build-to lines to define the transition between public and private zones.

Discussion

The creation of a continuous public realm is crucial to the development of North San José as a distinct place that is inviting to people. Along with building placement and orientation, consistency in the zone between the building and the street, which can range from public, semi-public, and semi-private in its use, is important to shaping this realm. Building elements such as entry zones, glazed façades, seating areas, stoops, and awnings; active ground-level uses; and sidewalks buffered from vehicular traffic with planting strips, parking spots, and trees; all welcome and encourage pedestrian activity, creating more vibrant, people-friendly places in North San José.

Some streets include build-to lines in order to define a continuous street edge. Defined building frontages that form a street wall can create an “urban room” feeling that enhances the pedestrian experience. Recesses and encroachments are permitted as outlined.



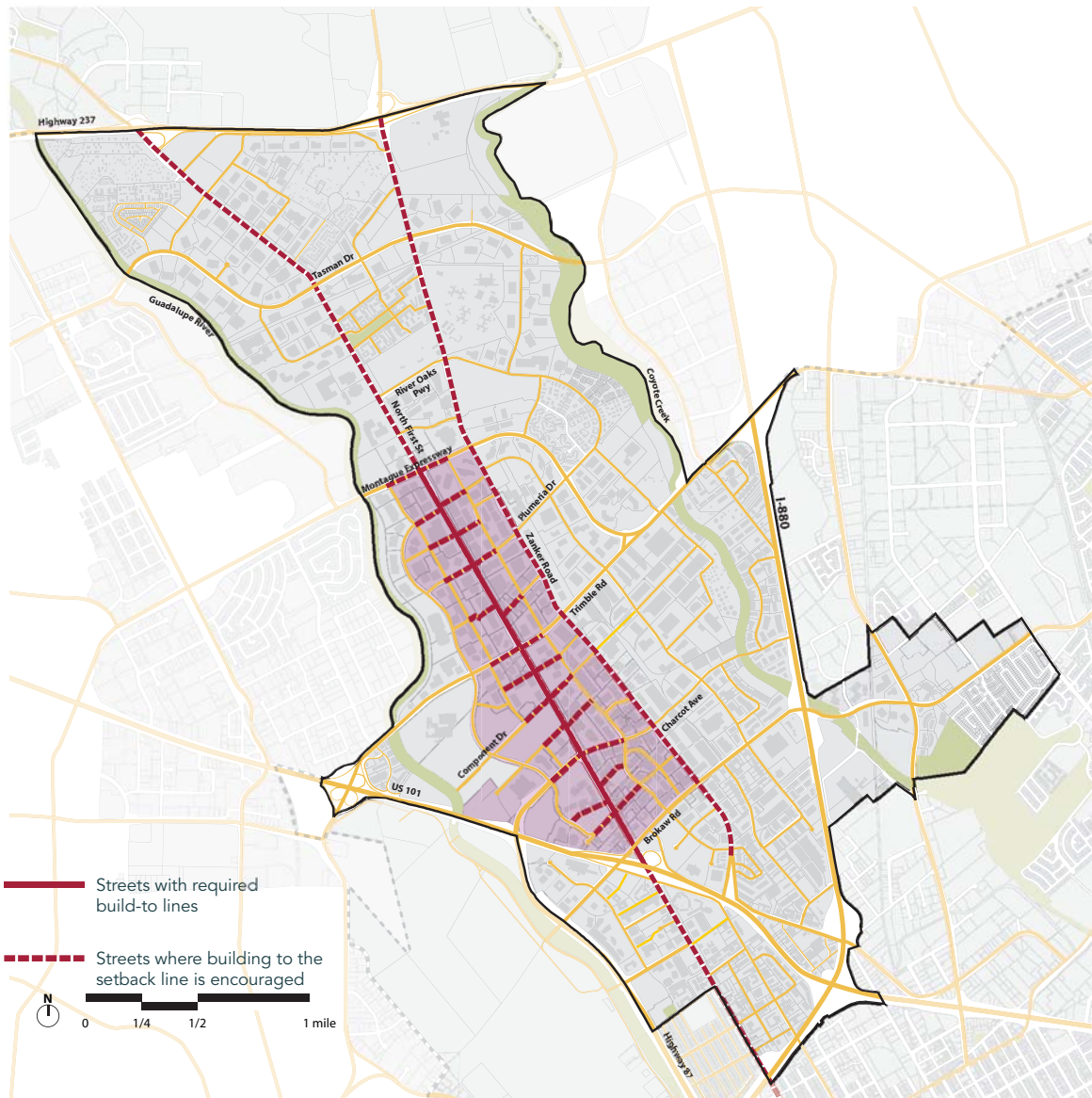
A continuous well-defined public realm is crucial to facilitate pedestrian activity. Build-to lines require buildings to form a continuous street edge that responds to the pedestrian scale.

Guidelines

- » At least 75% of a building’s street-oriented façade (measured by length) must meet the defined build-to line (where applicable) or main façade line. Permitted recesses and encroachments may be counted toward this requirement.
- » Build-to lines are established along the following streets (also see *Guidelines for the Streetscape: Street Hierarchies and Typologies*):
 - » North First Street within the Core Area;
 - » New Mixed-Use Retail streets; and
 - » New Residential streets.

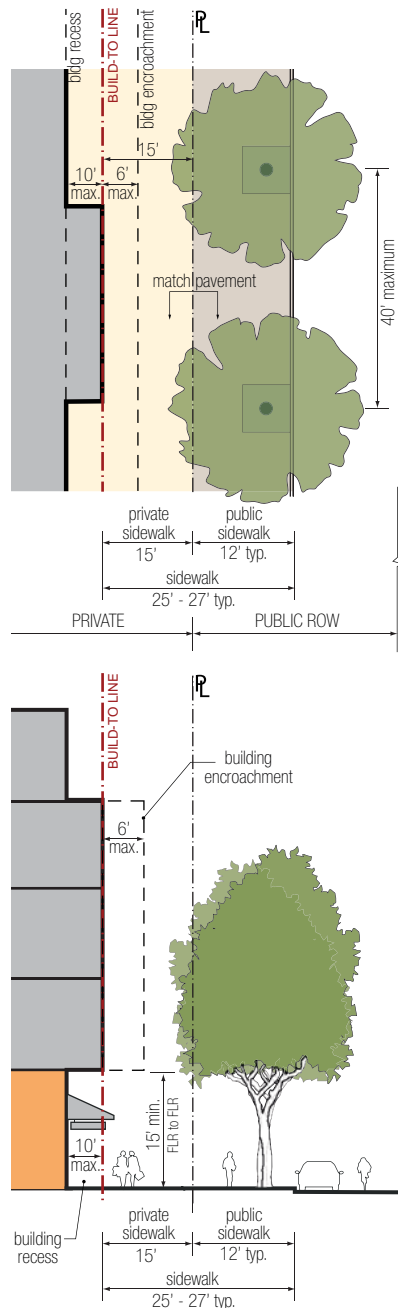


Pedestrian-Friendly Environment Sustainability
 Mix of Uses Identity



Existing and planned streets in North San José with build-to lines (also see *Guidelines for the Streetscape: Street Hierarchy and Typologies*).

Street Frontages



Primary Streets in the Core Area
Street Frontages

Guidelines - Core Area

General Guidelines

- » “Primary Streets” are defined as all Core Area streets or street segments that have a build-to line as shown in the diagram on the previous page.
- » Buildings should be oriented parallel to public streets and built to the build-to lines unless a publicly-accessible park or plaza is formed through an alternate placement of the building.
- » Taller buildings may be oriented perpendicular to primary streets as long as a building of lesser height is oriented parallel to the street edge (also see *Building Orientation and Siting*, earlier).
- » If a publicly-accessible park or plaza is created, it should be oriented to and accessible from the street. It may not be wider than 25% of the total block width or 200 feet, whichever is smaller (also see *Building Orientation and Siting*, earlier, and *Guidelines for Buildings: Building Frontages and Entries*).
- » Building recesses and encroachments are allowed as follows:
 - » Building recesses of up to 10 feet and building encroachments of up to 6 feet are allowed from the main façade line.
 - » Altogether, recesses and encroachments (measured by length) should not exceed 50% of the portion of the building’s street-oriented façade that meets the build-to line.
 - » Ground-floor recesses for entrances, lobbies, and display windows are encouraged.
 - » Encroachments may occur only at a height of 15 feet or more from the street level.
 - » Also see *Retail*, this section, and *Guidelines for Buildings: Building Frontages and Entries*.



Pedestrian-Friendly Environment
Mix of Uses
Identity

Sustainability

Guidelines - Core Area (continued)

Guidelines for Primary Streets in the Core Area

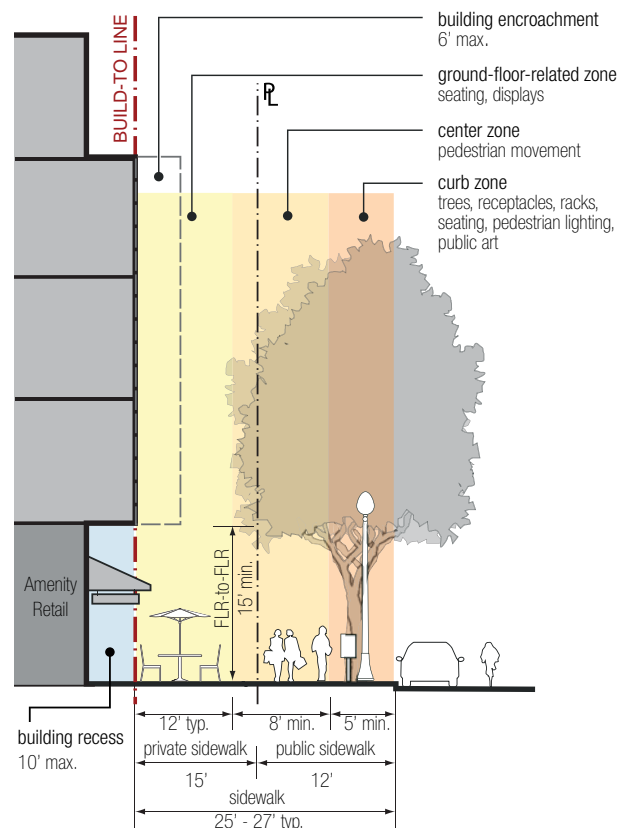
- » The build-to line is 15 feet from the street-facing property line.
- » The setback zone must be designed as a sidewalk from the building face to the edge of the public right-of-way.
- » The private portion of the sidewalk should be designed to the same standards as the public sidewalk in order to generate a seamless and uninterrupted pedestrian environment.
- » The sidewalk should be designed to include three different zones, as follows:
 - » The zone closest to the building, typically 12 feet wide, can be used for ground-floor-related activities such as café seating, displays, and entry areas.
 - » The center zone, at least 8 feet wide, is for free pedestrian movement.
 - » The curb zone, at least 5 feet wide, should accommodate street trees, lighting, and street furniture such as trash receptacles, benches, and bike racks.
 - » See *Guidelines for the Streetscape: Street Hierarchy and Typologies* for specific street dimensions.

Guidelines for other streets in the Core Area

- » Other streets in the Core Area have scaled-down sidewalk areas and different street frontage conditions. Please refer to *Guidelines for the Streetscape: Street Typologies* for specific dimensions on all Core Area streets or street segments that do not have a defined build-to line.

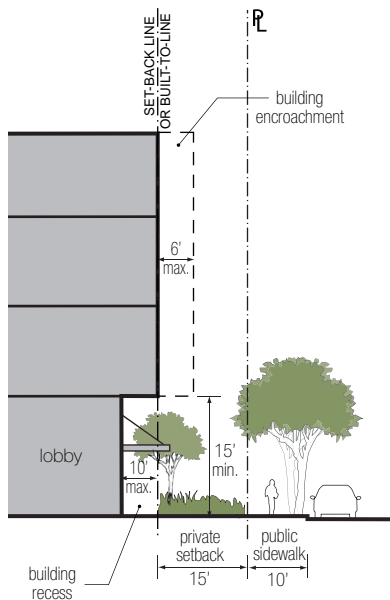
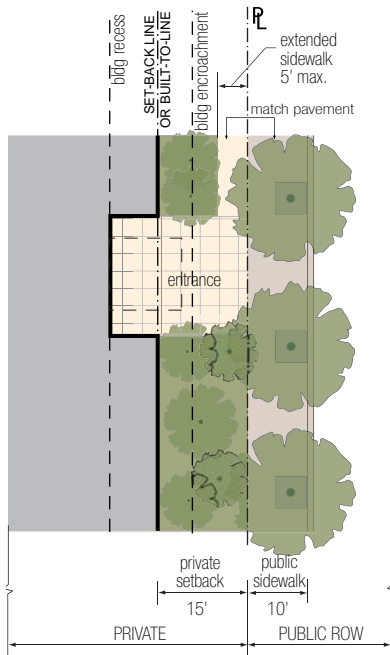


A widened urban sidewalk as envisioned along North First Street includes different zones for sitting, walking, and amenities.



This diagram shows a detailed view of the sidewalk zones along Primary Streets in the Core Area.

Street Frontages



Industrial Park/Light Industrial Area
Street Frontages

Guidelines - Industrial Park/Light Industrial Areas

- » Buildings should be placed parallel to the street. Small parking lots for visitors may be located between the street and building entrance, but large surface parking areas should be located behind the building or along the sides.
- » Orient all main entrances to the street.
- » Place buildings with more customer interaction, such as offices, along the street edge; place larger buildings with less customer interaction, such as production facilities, behind these buildings.
- » Buildings must be built to the build-to lines where applicable, such as areas along North First Street within the Core Area (see diagram earlier in this section). Where shown in the diagram, buildings are encouraged to be built to the setback line.
- » Building recesses and encroachments are allowed as follows:
 - » Building recesses of up to 10 feet and encroachments of up to 6 feet are allowed from the main façade line to increase building articulation.
 - » Altogether, recesses and encroachments (measured by length) should not exceed 50% of the portion of the building's street-oriented façade that meets the main façade line.
 - » Occasional recesses on the ground floor for entrances, lobbies, and service retail are encouraged.
 - » Encroachments may occur only at a height of 15 feet or more from the street level.
- » The public sidewalk can be extended on the private side. The setback zone should include a landscaped area at least 10 feet in depth, unless the zone is used to accommodate entry zones, small



Generous entrances, small plazas, green spaces and seating areas are encouraged along street frontage.



Pedestrian-Friendly Environment
Mix of Uses
Identity

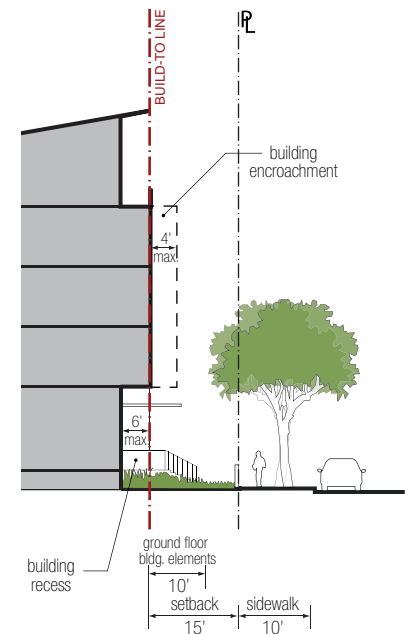
Sustainability

Guidelines - Residential Areas

- » Residential buildings in the Core Area should follow the Street Frontage guidelines for the Core Area (see previous pages).
- » The build-to line for residential buildings is 15 feet from the street-facing property line.
- » Building recesses and encroachments are allowed as follows:
 - » Ground-floor building-element encroachments of up to 10 feet, for projections such as stoops, porches, and patios, are allowed.
 - » Stoops are required along streets with street parking.
 - » Above the ground floor, building recesses of up to 6 feet and building encroachments of up to 4 feet are allowed from the main façade line for balconies, patios, and other elements.
 - » Altogether, recesses and encroachments (measured by length) should not exceed 50% of the portion of the building's street-oriented façade that meets the build-to line.
 - » Also see *Guidelines for the Streetscape: Street Typologies* and *Guidelines for Buildings: Building Frontages and Entries*.

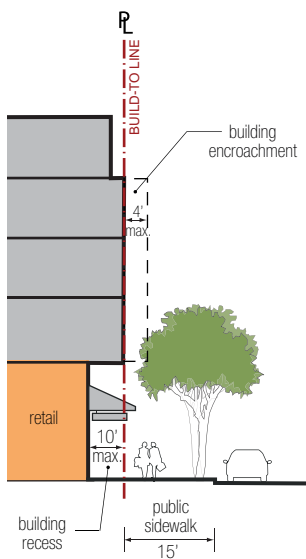
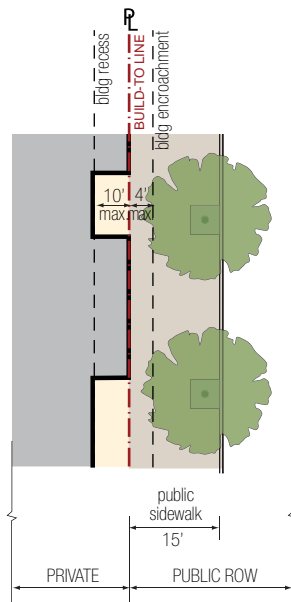


A residential setback creates space for planting, porches and trellises while creating a buffer zone between public and private.



Residential Area Street Frontages

Street Frontages



Mixed-Use Area Street Frontages
(outside of the Core Area)

Guidelines - Mixed-Use Areas outside the Core Area

- » Buildings should be placed parallel to the street in order to form a continuous street edge.
- » Vehicular access should be placed on side streets to reduce curb cuts. Small parking lots for visitors may be located between the street and building entrance, but large surface parking areas should be located behind the building or along the sides.
- » For retail uses that require “teaser” parking that is visible from the street, a small amount of parking may be accommodated at the side of the building.
- » In retail streets that are designed with diagonal street parking, additional on-site parking is discouraged unless located behind the building.
- » Orient main entrances and ground-floor-use entrances to the street.
- » Building recesses and encroachments are allowed as follows:
 - » Recesses on the ground floor for entrances, lobbies and display windows are encouraged.
 - » Above the ground floor, building recesses of up to 10 feet and encroachments of up to 4 feet are allowed from the main façade line for balconies, patios, and other elements.
 - » Also see *Guidelines for Site Planning: Retail, Guidelines*



A continuous street frontage and generous curb-cut-free sidewalk are important in creating a welcoming pedestrian environment in mixed-use areas.



Pedestrian-Friendly Environment Sustainability
Mix of Uses
Identity

Retail

Create places with retail that encourage and strengthen pedestrian activity, and that are closely linked to public transit and other forms of transportation.

Discussion

Residential neighborhoods and employment centers should fully integrate the functions that build community: home, work, institutions, recreation, and retail. Retail districts, if designed well, can help create a center for a neighborhood, providing a place not only to shop, but also to meet friends and gather as a community. To create successful sustainable retail districts in North San José, sites should be accessible by multiple modes of transportation and should meet the Retail Site Criteria provided here. Retail should be designed with the pedestrian scale and experience in mind in order to create places where people want to linger and spend time. Buildings should be oriented to the street and organized around a variety of open spaces of different scales to provide spots to pause and relax.



Design stand-alone neighborhood retail as retail streets that include street parking, trees, sidewalks, and multiple pedestrian crossings.



Retail areas should create a place for people; in addition to shops, they should include gathering areas where people can meet and play.



Pedestrian-Friendly Environment
Mix of Uses
Identity

Sustainability
Public Art

Four Types of Retail Anticipated in North San José

Amenity Retail

These are groups of shops and food establishments that typically occupy as much as 20,000 square feet or more as part of a mixed-use development that is primarily an employment or residential center. Amenity retail meets the Retail Site Criteria (next page), has enough retail variety to attract customers from within and beyond the project boundaries, and adds retail variety and convenience to North San José.

Neighborhood Retail Centers

These are the equivalent of the supermarket and drug shopping center, and can be in a mixed-use or a stand-alone format. They typically have at approximately 130,000 square feet of retail space and include restaurants as well as a variety of shops. In North San José these centers are likely to be augmented or co-located with public facilities and uses.

Regional Shopping Center

There is an opportunity at the intersection of Highway 237 and Interstate 880 to create a retail destination that offers a large selection of retail goods and services in 600,000 square feet of space or more. This retail center would draw customers from North San José and beyond.

Service Retail

Small retail shops or snack bars under 1,300 square feet in size that cater to the employees of a specific building, business, or complex, do not seek customers from a larger area, are not visible from the street, and do not have signage on the street, are considered service retail. This type of retail is not addressed here.

The following guidelines focus on Amenity Retail, Neighborhood Retail Centers, and Regional Shopping Centers. These will contribute much to the desirability, placemaking, and sense of community in North San José. To build upon North San José's identity as a cutting-edge 21st-century neighborhood, retail buildings and sites should be designed with customer convenience and experience, and environmental benefits including green building practices, in mind. Furthermore, retail areas will benefit from flexibility in layout and service capacity to meet future retailer needs, allowing the retail to change over time with market trends. Together these features will help ensure that North San José retail will be successful now and well into the future.



Create pedestrian-friendly environments that include shading and outdoor seating.

Retail



Public art can enliven retail districts.



Public art can be integrated in stand-alone shopping centers and can enhance large open spaces such as parking lots.

Guidelines

Retail Site Criteria

- » Use the following Retail Site Criteria to ensure the highest likelihood of retail success over time:
 - » Sites must have high visibility and excellent accessibility, including but not limited to gateway or prominent intersection locations.
 - » Sites should have strong market demand with the ability to draw customers from beyond the project itself.
 - » Encourage and build strong relationships between proposed retail and target customer populations such as proximity, orientation, public transit, bike and pedestrian trails.
 - » Consider the relationship and proximity between proposed retail and existing retail on adjacent sites. Where possible, create synergy, connections, and placemaking rather than scattered, disconnected retail locations.
 - » Strong potential retail sites are discussed in the Retail Report that is part of the *North San José Implementation Plan*.

- » Encourage retail placemaking to create a globally competitive environment.
- » Encourage mixed-use projects that include amenity retail spaces, needed within neighborhood and employment centers, and neighborhood retail centers.
- » Provide easy pedestrian, bicyclist, and motorist access to the site and into the retail area so the customer experience is fluid.
- » Provide approximately 10% of the retail parking through teaser parking immediately adjacent to retail or on-street parking. Place the balance of parking in structures behind retail buildings.
- » Encourage high-quality building design:
 - » Encourage retail frontages to express a distinct personality, engaging the customer and contributing to placemaking.
 - » Discourage themed building ensembles.
 - » Also see *Guidelines for Buildings: Building Design and Materials* for more detailed guidelines.



Pedestrian-Friendly Environment
Mix of Uses
Identity

Sustainability
Public Art

Guidelines (continued)

- » Encourage well-designed, quality community gathering spots and outdoor spaces that range from calm to active in nature as a part of retail areas.
- » Encourage main street concepts for stand-alone neighborhood retail:
 - » Successful main street retail only works when placed on both sides of the street with many pedestrian crossings, and on streets that are 60 to 76 feet wide.
 - » Two-sided retail along wider streets is discouraged unless the retail is part of a dense population center that provides a critical mass of people at future build-out.
 - » Also see *Street Frontages*, in this section, and *Guidelines for the Streetscape: Street Hierarchy and Typologies*.
- » Retail space should be designed with flexibility to adapt to market changes over time. This includes accommodating different sizes and combinations of tenants, and providing utilities, grease interceptors, vents, trash storage, and service provisions that allow for future tenant mix.
- » Design pathways, open spaces, and buildings to the pedestrian scale in order to encourage pedestrian activity and to connect retail areas with residential neighborhoods and employment centers.
- » Include benches, seating, and public art along pathways and streets.
- » Utilize public art to enhance the uniqueness of the retail environment. Introduce “hot spots” (wireless/electrical) and “strong spots” (flexible installation platforms) to accommodate temporary art.



One of several placemaking studies on a hypothetical site that illustrates a retail street anchored by a library and community center mixed with housing and a community park.

Transitions

Guide the transition between existing and new development, particularly in cases with a change in scale and use, to create a continuous urban fabric.

Discussion

As North San José's landscape continues to evolve, it is inevitable that new development, of a different scale and character than existing development, will be planned and built. By giving careful consideration to the design of the transition zone between different developments, possible conflicts between scale and uses can be avoided and a desirable, human-scaled environment can be maintained.



Carefully designed transitions between abutting properties can contribute to the overall connectivity of North San José and create usable and pleasant space.



Connectivity
Mix of Uses

Identity

Guidelines

New Residential Development, Adjacent to Existing Residential less than 35 feet in height:

- » New multi-story residential development adjacent to existing residential development with a building height of less than thirty-five (35) feet must have a setback from the shared property line of at least one foot for every foot of building height with a minimum setback of fifty (50) feet (Figure 1).
- » A residential development is defined as an *existing* development if construction was completed and the Certificate of Occupancy (COO) was issued by the end of 2007.
- » New development must comply with the height limits and densities set forth in the North San José Development Policy.
- » In cases where a new private street is planned between the proposed development and the adjacent property, the setback should accommodate the dimensions needed to implement the new street. In cases where a new street is not proposed, the setback area should be designed to accommodate a paseo that allows for pedestrian movement.
- » In addition to or in place of a street, landscape plantings that include large trees and dense vegetation should be used to provide significant visual screening along the entire length of the shared property line. If fences or walls are proposed along this line they must not exceed six (6) feet in height.

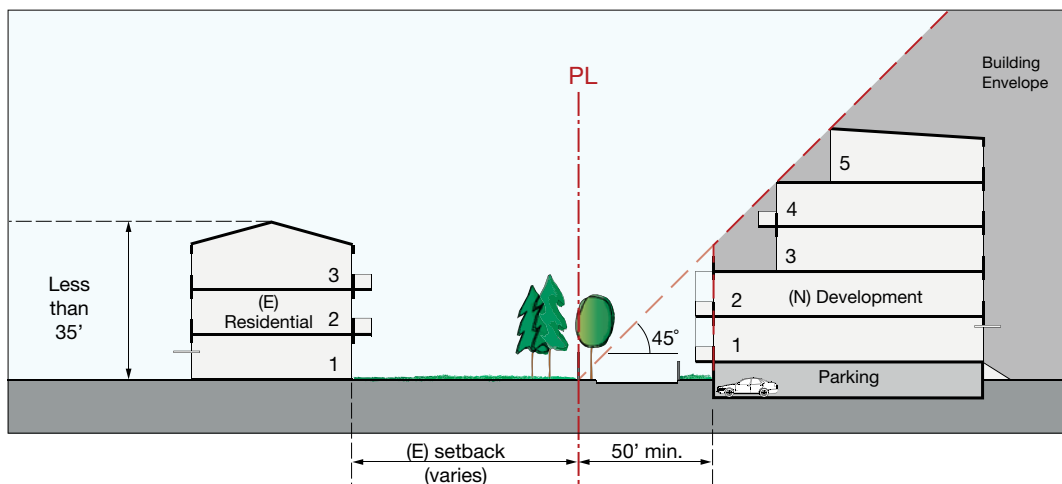


Figure 1: Transition areas for new residential development adjacent to existing residential that is less than 35 feet in height.

Transitions

Guidelines (continued)

New Residential Development, Adjacent to Existing Residential more than 35 feet in height:

- » New multi-story residential development adjacent to existing residential development with a building height of more than thirty-five (35) feet must have a setback from the shared property line of at least one foot for every foot of building height with a minimum setback of thirty (30) feet, or a minimum distance of sixty (60) feet from building face to building face (see Figure 2).
- » A residential development is defined as an *existing* development if construction was completed and the Certificate of Occupancy (COO) was issued by the end of 2007.
- » New development must comply with the height limits and densities set forth in the North San José Development Policy.
- » In cases where a new private street is planned between the proposed development and the adjacent property, the setback should accommodate the dimensions needed to implement the new street. In cases where a new street is not proposed, the setback area should be designed to accommodate a paseo that allows for pedestrian movement.
- » In addition to or in place of a street, landscape plantings that include large trees and dense vegetation should be used to provide significant visual screening along the entire length of the shared property line. If fences or walls are proposed along this line they must not exceed six (6) feet in height.

New Residential Development, Adjacent to Non-Residential (Rear and Side Setbacks):

- » New multi-story residential development adjacent to existing or planned non-residential development should have a minimum setback of thirty (30) feet from the shared property line. Between 30 and 50 feet from the rear property line, the setback is defined by a line that follows a 45-degree angle from the property line, resulting in a 1-to-1 ratio of height to width, in this zone (see Figure 3).
- » This guideline is intended to reduce the impacts from adjacent uses on light, air, and sound conditions for the new residential development.



Connectivity
Mix of Uses

Identity

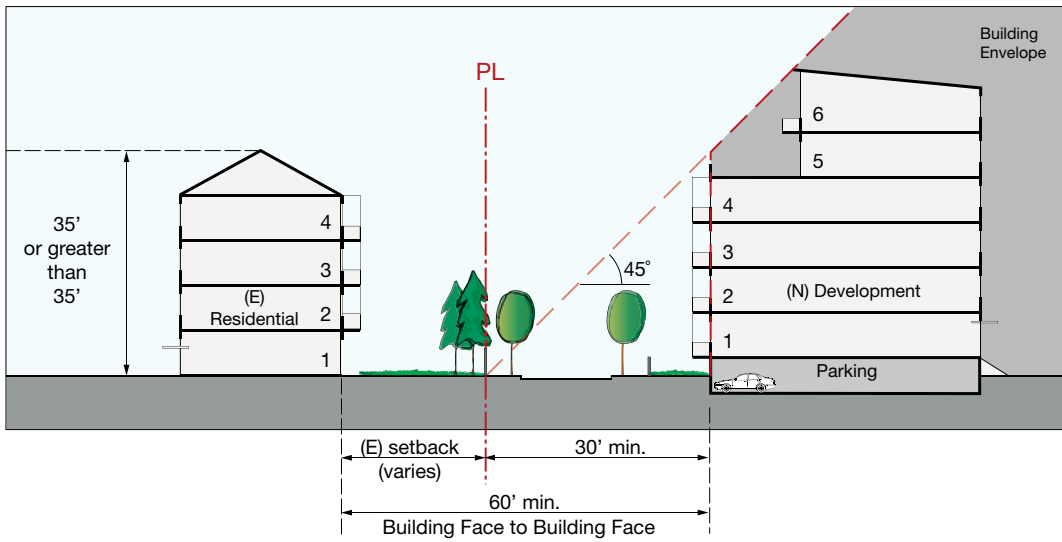


Figure 2: Transition areas for new residential development adjacent to existing residential that is more than 35 feet in height.

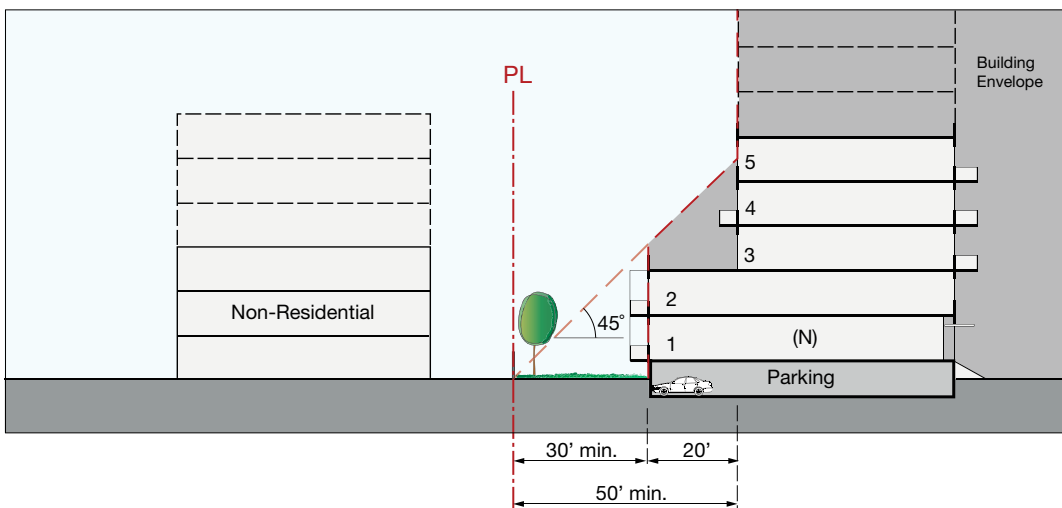


Figure 3: Transitions areas for new residential development adjacent to existing or planned non-residential development.



Guidelines for Buildings

Building Massing

Building Articulation

Building Frontages and Entries

Building Design and Materials

Building Massing

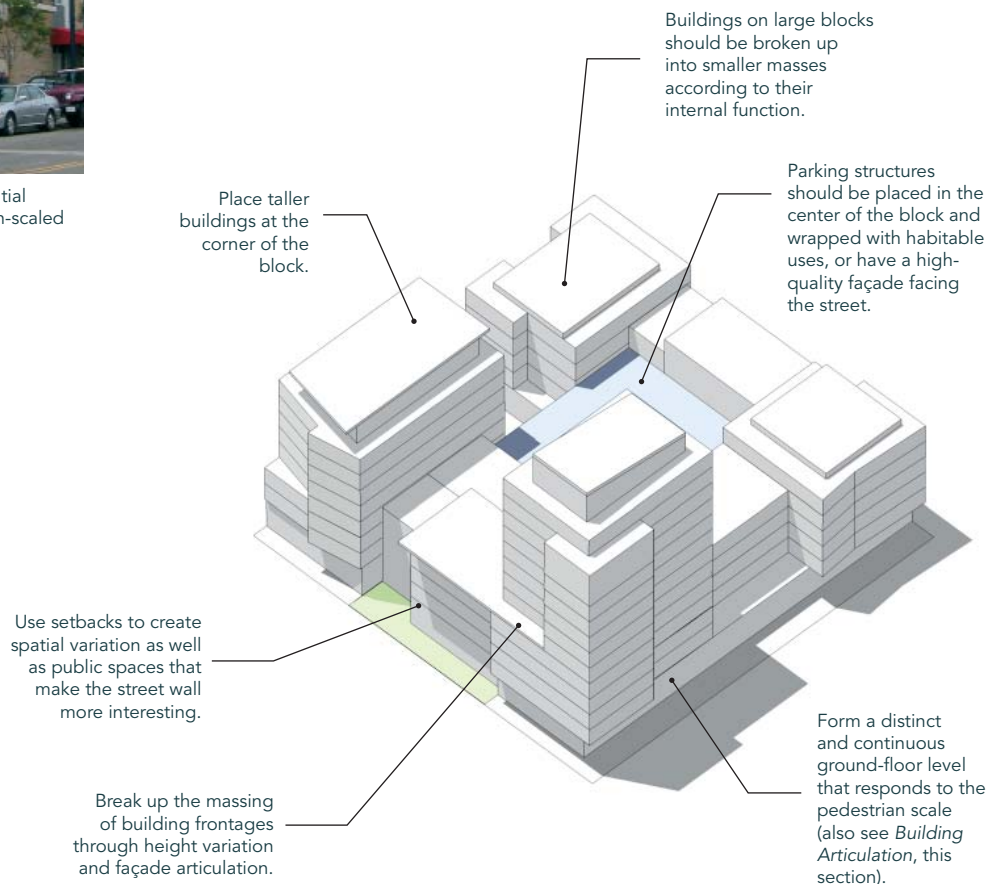
The careful massing of buildings contributes to the distinct character of North San José as an urban, pedestrian-oriented place to live and work.



The articulation of masses in residential buildings helps to generate a human-scaled built environment.

Discussion

The massing of a building has a significant impact on how the size of that building is perceived by a person at the street level. By breaking up a large building into smaller masses, the building's apparent bulk can be reduced, forming a more interesting and varied street wall. Special attention should be paid to building masses that face onto public streets, to relationships between neighboring buildings, and to potential views from taller buildings, when considering building massing.





Pedestrian-Friendly Environment Identity
Mix of Uses

Guidelines

- » Minimum spacing between residential towers should be 80 feet.
- » Place taller portions of a building at the building corners.
- » If a building is in the sun path of a public open space, place the tower to minimize shading of that space.
- » New buildings facing North First Street in the Core Area should be at least 4 stories in height and preferably taller.
- » Buildings along North First Street in the Core Area should form a street wall composed of buildings of similar heights. Accent towers should be placed at major intersections (also see *Building Heights*). Interruptions in the street wall should take the form of publicly-accessible parks or plazas (also see *Site Layout: Private Pocket Parks and Plazas* and *Street Frontages*, this section).
- » The apparent bulk of buildings should be broken up into smaller masses that reflect a building's internal functions.



A large building's apparent bulk can be reduced when the massing is broken up and generates an attractive ground-floor level. Emphasized corners help to spatially frame intersections.

Building Articulation

Articulate building volumes and façades to create a diverse built environment that responds to the pedestrian scale, and that clearly defines public-to-private transitions.

Discussion

To reduce building scale and bulk as well as to make the overall urban environment visually interesting, buildings should be well-articulated. Building articulation means that a building is physically modulated, particularly along its exterior elevations, through the use of massing changes, recesses, protrusions, overhangs, arcades, material changes, façade rhythm and layering patterns, decorative elements, shading devices, and roof forms. All of these elements can be used to give the building a finer grain that responds to the human scale. Building articulation should respond to the scale and design of the existing context, including adjacent buildings, open spaces, and the streetscape.



Office and mixed-use buildings should have a clearly articulated ground, middle and top zone. Held corners or small public spaces at corners help to enrich the pedestrian environment.

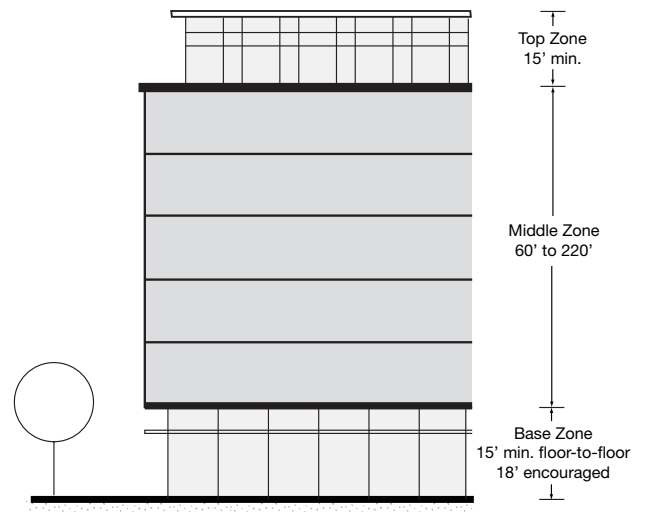


Pedestrian-Friendly Environment
Mix of Uses

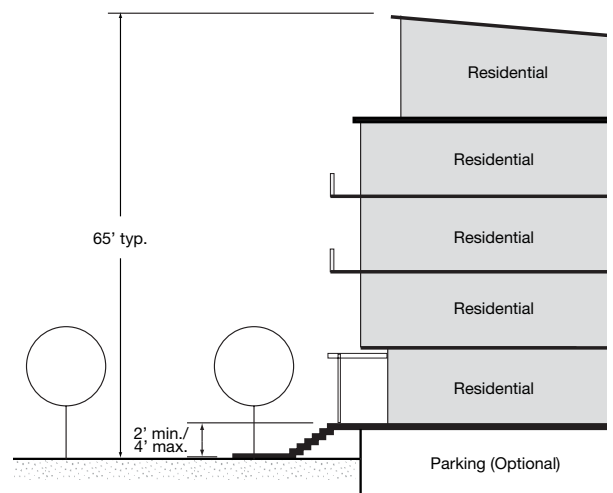
Identity
Sustainability

Guidelines

- » Promote fine-grained development and building variety along retail frontages and in residential areas to create a pedestrian-friendly environment.
- » Encourage horizontal and vertical building articulation through changes in building volumes, recesses, balconies, awnings, textures, materials, and colors.
- » Emphasize building corners by projecting out or forming small public spaces through recesses.
- » Office and mixed-use buildings should have a clearly articulated ground floor and roof zone. Buildings over 65 feet in height should have a distinguishable base, middle, and top zone.
- » The ground-floor level of a residential building facing a street should be elevated by a minimum of 2 feet and a maximum of 4 feet.
- » Residential units located at grade (for example, those that open onto mid-block pathways) should have a carefully designed transition zone that can accommodate front yards, porches, steps, patios, or stoops when facing public spaces such as streets, paseos, plazas, or courtyards (also see *Guidelines for Site Planning: Street Frontages*).
- » Building façades should be articulated to reduce wind effects.
- » Encourage the use of shading devices on building façades, particularly on south- and west-facing façades, to reduce solar heat gain within buildings.



Office and mixed-use buildings should have an articulated base, middle, and top zone.



The ground-floor level of residential buildings facing a street should be elevated by a minimum of 2 feet and a maximum of 4 feet.

Building Frontages and Entries

Buildings should be oriented to and accessed from a public street or pathway to promote pedestrian activity, transit use, and retail commercial activity.



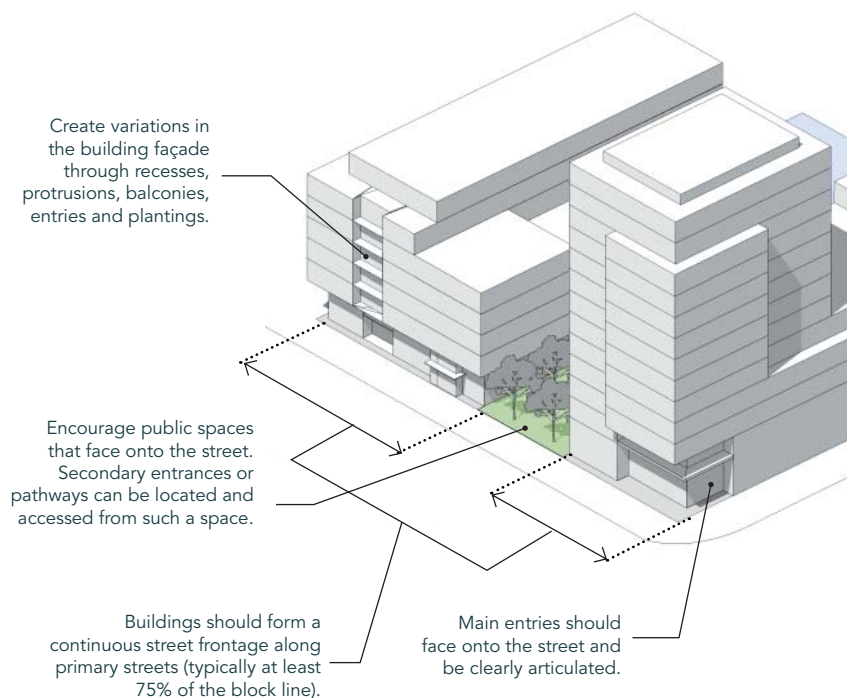
Building entrances should be clearly articulated and should face onto public streets or pathways.



Generous lobbies facing onto public streets look inviting, activate the pedestrian realm, and help with orientation and wayfinding.

Discussion

To create an interesting and appropriately-scaled pedestrian environment, buildings should face the street and create a continuous street wall. Large openings such as surface parking lots along a street increase perceived distances and reduce the comfort level of pedestrians due to lack of activity. Building façades should include a variety of well-articulated recesses, protrusions, balconies, awnings, and façade elements to make the façade visually interesting and responsive to the human scale. Main entries should be visible and accessible from a public street. Generally, the ground-floor level should be clearly defined and form a continuous edge to encourage pedestrian activity, especially when it contains retail uses.





Pedestrian-Friendly Environment
Connectivity
Identity

Guidelines

- » The main façades of buildings should generally be oriented parallel to public streets or pathways.
- » The long sides of taller buildings may be oriented at a 90-degree angle to the street, particularly along streets that have heavy car traffic, provided that:
 - » a main façade of a shorter height, typically between two and six stories, is oriented parallel to the street; and
 - » direct pedestrian pathways to building entrances or public spaces are provided.
- » Main entries should be visually prominent and should be oriented to a public street or pathway.
- » Double-height and transparent entry lobbies are encouraged for residential, mixed-use, and office buildings.
- » Encourage secondary entrances for buildings that face onto a secondary street, pathway, or paseo as well as onto a public street.
- » Residential units at grade and facing a street should have an elevated ground-floor level (see *Building Articulation*, this section).
- » Encourage retail frontages to express a distinct personality, engaging the customer and contributing to placemaking.
- » Minimize the visual impact of service areas and garage entrances by locating them behind buildings and away from public streets and pathways. Provide screening through landscaping, fences and canopies (also see *Guidelines for Parking*).
- » Buildings facing trails can have trail-accessible entrances or backyards provided that the minimum setback zone is landscaped.



In residential buildings, frequent entrances should face onto public streets or pathways.



If taller buildings are not oriented parallel to the street, a shorter building can form the street wall.

Building Design and Materials

Promote world-class architecture, particularly along North First Street and in residential areas. Encourage the use of high-quality, sustainable building materials and systems.



Green roofs improve insulation values, filter rainwater, and can be used as open space.



Exterior shading elements can be used as a key design feature of a building façade.



Naturally vented double-skin façades improve thermal energy performance.

Discussion

North San José presents the opportunity to showcase architecture that reflects the spirit of innovation of the Silicon Valley. Good building design can greatly contribute to the character and livability of North San José. The use of high-quality materials and sustainable building methods can respond to the demands of people's needs today and also significantly reduce impacts on the natural environment. This is important from the global as well as the local perspective since a successful, high-quality district will attract people and businesses, and serve as a model for future development.

Guidelines

- » Allow for a variety of architectural styles, encouraging contemporary styles that tie into North San José's identity as a place of innovation.
- » Investigate opportunities to reuse existing buildings for new development.
- » Prefer long-lasting and low-maintenance façade materials such as metals, glass, brick, engineered wood, concrete and stone. Use light colors for large façade areas.
- » On the façades of large buildings, use a balanced mix of materials.
- » Use generous roof overhangs and awnings for shading.
- » In cases where roofs will be visible from above, green roofs or non-reflective materials in neutral colors should be used.
- » Utility areas and boxes should be located out of sight from public streets and pathways and should be integrated in the overall design.



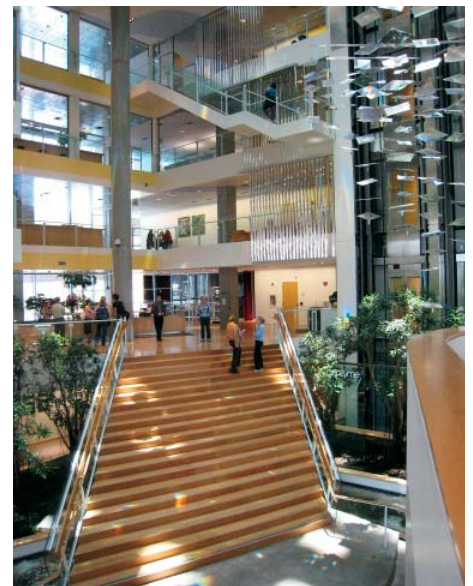
Pedestrian-Friendly Environment
Identity
Sustainability

Guidelines - Office Buildings

- » Utilize the North First Street Corridor as an opportunity to showcase elegant, contemporary design and compelling art to support the vision for North San José as an innovative urban place.
- » Design a distinct and continuous ground-floor level with a high percentage of clear glass that enriches the pedestrian environment.
- » Discourage blank walls, particularly at the ground-floor level. Blank walls should be textured or layered with different materials, or should incorporate art enhancements where appropriate.
- » Encourage the use of public art above the street level, such as pieces that involve cladding elements and skyline delineation.
- » Encourage maximization of daylighting through narrower building widths, skylights, atriums, light baffles, glazed northern façades, and shaded southern façades to reduce reliance on artificial lighting.
- » Encourage increased shading through controlled louvers, canopies, fabrics, blinds, or electrochromatic glass on south- and west-facing façades to reduce building heat gain.
- » Discourage the use of reflective glazing and other materials that may have potential negative impacts upon adjacent properties.
- » Encourage operable windows or ventilated double façades to allow for natural ventilation.
- » Mechanical units on roofs should not be visible from the street level. Provide screening if units are visible from taller buildings.



Encourage innovative office building design.



Atriums can provide light and ventilation and also serve as informal meeting spaces.



Utilize shading devices to minimize solar heat gain and optimize natural daylight use.

Building Design and Materials



Color variations, architectural modulations, and shading devices can reduce the perceived scale of large buildings.

Guidelines - Industrial Buildings

- » Buildings with large footprints should include façade elements and materials that make the building elevation more visually interesting, particularly along streets and pathways.
- » Place the most active functions, such as office spaces or customer areas, along public streets.
- » Design building volumes and façade portions differently to reflect their varying internal functions.
- » Use neutral colors and non-reflective materials on roofs when they are visible from above.
- » Encourage contemporary design and colors, and the use of innovative art.
- » Mechanical units, including roof units, should not be visible from the ground-floor level; or, they should be screened with architectural panels.
- » Encourage the visibility of internal activities unless security concerns prevent transparency.
- » Use modular systems to increase functional flexibility.



Transparency in industrial buildings can showcase activities and contribute to the high-tech character of North San José.



Different façade materials and shapes can transform even large production and distribution facilities into interesting architecture.



Pedestrian-Friendly Environment
Identity
Sustainability

Guidelines - Residential Buildings

- » Provide a mix of unit sizes and typologies.
- » Encourage contemporary design and innovative layouts that reflect the character of North San José, and allow for a variety of styles, colors and materials even within a single development.
- » Integrate shared open spaces such as courtyards, patios, community gardens, playgrounds, community gathering spaces, and other amenities into the overall design.
- » Encourage smaller building depths and wider unit frontages to maximize daylighting and open space.
- » Incorporate modulations of building volumes and architectural details such as recesses, protrusions, balconies, awnings, exterior shading devices, window reveals, floor height changes, parapets, and varying roof forms to add visual interest and three-dimensional articulation.
- » Include artist-designed building elements to give buildings a unique character.
- » If sustainable features such as green roofs, shading devices, solar panels, or water collection systems are used, integrate them in the overall design and make them a distinct and visible feature.
- » Prefer lighter colors and bold accent colors as well as a mix of different materials that contribute to façade layering and variation.
- » Avoid blank and windowless walls; or, use architectural elements and textured surfaces and materials to reduce their visual impact.
- » Provide transition zones between private and semi-public spaces through the use of landscaping, fences, trellises, walls, or a change in floor elevation.



Use vertical façade modulation, floor height variation, and a mix of materials to create architectural interest.



Ground-floor units should include well-designed transition zones between the public and private realm.

Building Design and Materials



Clearly-articulated ground-floor retail in a mixed-use residential building enhances the pedestrian realm.



Street-facing ground floors with double-height spaces and regularly spaced vertical columns contribute to the quality of the streetscape.

Guidelines - All Mixed-Use Buildings

- » Mixed-use buildings that include ground-floor retail or other commercial uses such as small offices should face a public street and form a continuous edge along the build-to line or setback line.
- » Provide frequent entrances into buildings and particularly into ground-floor uses.
- » Design the building frontage to reflect internal functions. For example, make retail, residential, and office uses legible from the outside through the appropriate use of window sizes and materials.
- » The ground floor should be designed to the pedestrian scale through the use of vertical architectural elements, awnings, entrances, signage, windows, and displays. Façade variations in the form of recesses or encroachments are encouraged to increase visual interest (also see *Guidelines for Site Planning: Street Frontages* for specific dimensions).
- » The design of the ground-floor façade should be integrated into the overall building's façade design; it should be compatible with the proportions of surrounding building elements and with the proportions of adjacent buildings.
- » Provide sufficient shading along pedestrian-oriented ground-floor façades.
- » At least 60% of the ground-floor façade should be glazed with clear, untinted glass. If double-paned glazing is used, it should be anti-reflective.
- » Avoid blank or windowless walls, particularly on the ground floor. Where blank walls are unavoidable, their visual impact should be mitigated through the use of seating, public art, landscaping, façade textures, or architectural elements.
- » Provide a maximum clear space between columns and a minimum number of columns within ground-floor activity spaces. Shear walls should be located along common interior walls.
- » The minimum ground-floor height is 15 feet from floor to floor.



Pedestrian-Friendly Environment
Identity
Sustainability

Guidelines - Retail in Mixed-Use Buildings

- » Place ground-floor retail along streets that meet the Retail Site Criteria (see *Guidelines for Site Planning: Retail*); design it to promote high levels of pedestrian activity.
- » All retail areas should have at least one entrance from the street, and the retail area as a whole should connect to parking, adjacent developments, upstairs tenants, ground-floor lobbies, and building elevators in a convenient and inviting way.
- » Guidelines for projects that contain 8,000 square feet or less of contiguous retail space are as follows:
 - » The minimum depth is 45 feet. Where possible, 60-foot depths are encouraged to accommodate a wider range of tenants, especially food tenants.
 - » The minimum floor-to-floor height is 15 feet. Where possible, 18-foot floor-to-floor heights are encouraged.
- » Guidelines for projects that have more than 8,000 square feet of contiguous retail space are as follows:
 - » The minimum depth is 60 feet, and the minimum floor-to-floor height is 18 feet, to accommodate and encourage larger stores as well as small shops, and to build flexibility for demising opportunities over time.
 - » If large retail spaces (8,000 square feet or more) are anticipated, 20-foot floor-to-floor heights or more are encouraged.
- » Retail located at building corners is encouraged regardless of the amount of retail space.
- » At least 60% of ground level store fronts should be glazed with clear, untinted glass. If double-paned glazing is used, it should be anti-reflective.
- » Storefronts should include lighting, awnings, signage, display windows, and entrances.
- » Minimize the number and massing of columns along or in front of the façade (also see *Guidelines for Site Planning: Street Frontages*, the *San José Redevelopment Agency's Signage Master Plan*, and the *City's Sign Ordinance*).



A modulated ground floor that accommodates retail breaks up the building mass and contributes to the pedestrian experience.



The back of a mixed-use building should be carefully designed. This example includes an alley, enclosed service areas, and a second store entrance.

Building Design and Materials



To promote pedestrian activity, the ground floor of a mixed-use retail building should include a high amount of clear glass along store fronts, lobbies, and other actively used spaces.



Retail spaces should visibly wrap around corners of buildings.

Guidelines - Retail in Mixed-Use Buildings (cont'd.)

- » Signage addressing both motorists and pedestrians should be part of the building design.
- » Service areas such as storage, trash, and mechanical areas should be located in enclosed areas on the back side of retail spaces.
- » Trash areas should be accessible from a secondary street or alley. Minimize noise and visual impact upon other uses through screens, façade detailing, and landscaping; seal and conceal trash areas to contain smells.
- » Loading docks, where required, should be at least 60 feet long and service corridors should be at least 6 feet wide. Minimize their street frontage, and minimize views from public areas, including streets.
- » Unless there is underground parking, the ground-floor level should be left unfinished in order to accommodate utilities and leasing flexibility.
- » Concrete slabs should have a depression along the perimeter of the building to permit accessible entry doors in various locations. Outside ramps or entry terraces are discouraged.
- » Grease interceptors, sewage lines, vent shafts, and trash enclosures should be anticipated and possibly built to provide for future tenant needs and avoid costly retrofits.
- » Utilities should be conveniently located and accessible from the tenant space, but should not be visible from pedestrian areas, including the street.
- » Integrate and centralize mechanical, electrical, and plumbing utilities for different uses within buildings.



Pedestrian-Friendly Environment
Identity
Sustainability

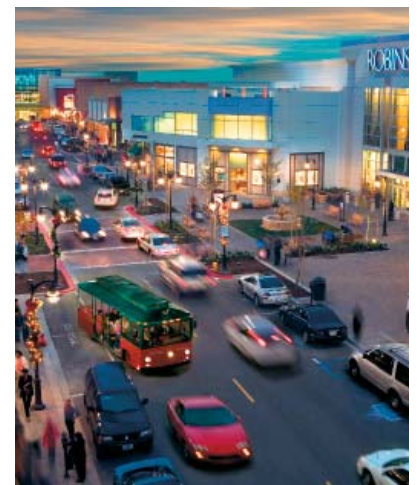
Guidelines - Retail Buildings

Refer to the *Retail in Mixed-Use Buildings* guidelines except for the following guidelines:

- » With the exception of big-box buildings, retail buildings should face onto public or private streets rather than parking lots.
- » Designs should allow pedestrians to move quickly from parking areas to storefronts.
- » Place sheltered bike racks close to the main entrance.
- » Accommodate small plazas, seating or landscaped areas in front of the buildings.
- » Utilize public art to provide character to the street experience.
- » Encourage contemporary design and the use of high-quality materials to contribute the overall character of North San José.
- » Develop site-specific public art to reinforce building presence and engagement in the community.
- » The façades of large retail buildings (greater than 10,000 square feet in area) should be modulated, particularly along street frontages or main building fronts, through a combination of varying heights, recesses or protrusions, architectural elements, and/or a mix of façade materials.
- » Provide large roof overhangs or separate pavilions for shading.
- » The minimum depth of in-line retail shops is 45 feet, and the minimum floor-to-floor height is 15 feet.
- » When a single contiguous retail space exceeds 8,000 square feet in area, the minimum depth should be 60 feet and the minimum floor-to-floor height should be 18 feet to accommodate larger retail stores.
- » Use a high percentage of glazing in building fronts. At least 80% of the main façade of small stores should consist of clear glazing.
- » Consolidate service areas, storage and trash areas, loading docks, and utilities out of the sight of pedestrians. Seal and conceal trash to confine smells.



The front of large retail buildings should include a prominent entrance, shading, and seating or vending areas.



Retail buildings should face onto a street and can include variations to form public spaces.

Building Design and Materials



Encourage unique design and high-quality materials for public buildings.



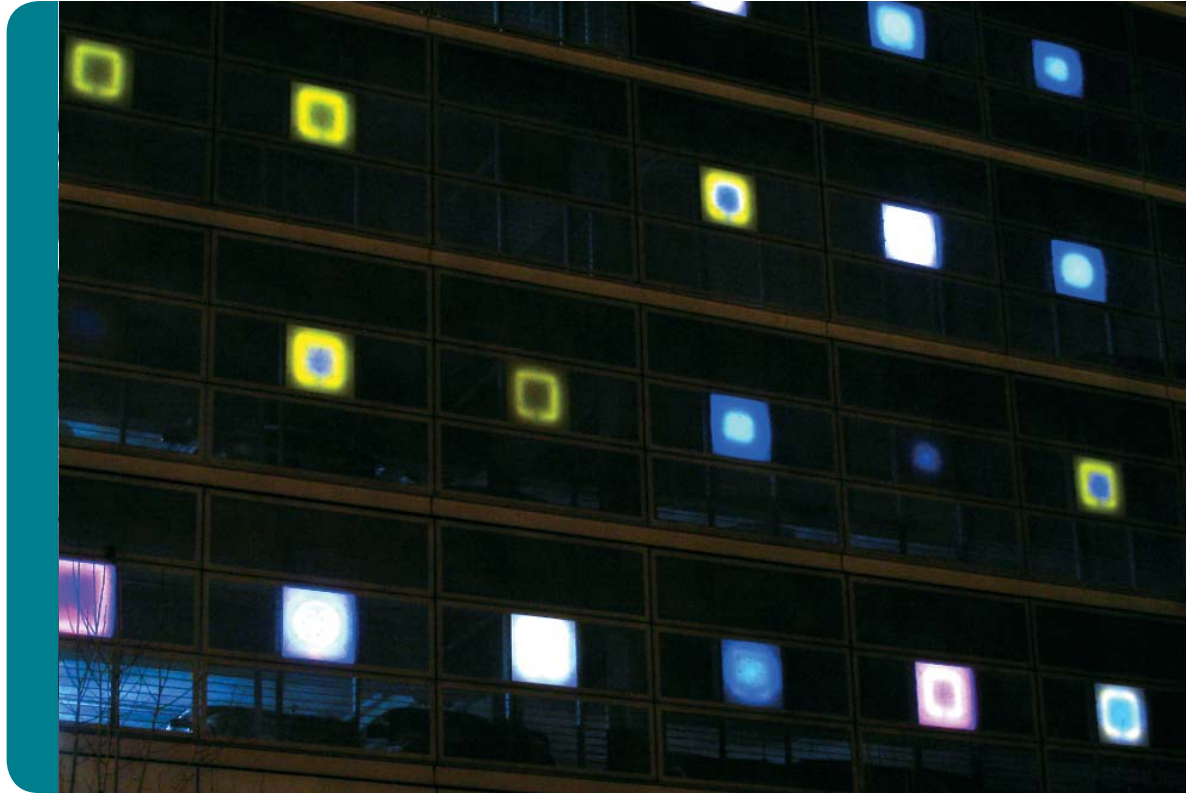
Schools or cultural institutions can reuse existing buildings or can be built as infill development.

Guidelines - Public Buildings

- » Public buildings should face onto a public street and include a clearly visible and articulated, preferably double-height entrance.
- » Public buildings can set a city standard for sustainable development and architectural quality.
- » Public buildings should be designed to reflect an open and inviting character:
 - » Encourage public buildings whose internal activities are visible from the exterior.
 - » Include a high percentage of glass, light colors, and high-quality materials in the main front façade.
 - » Design different volumes and portions of the building to reflect their differing internal functions.
- » Encourage public buildings that have an urban character - for example, make them multi-storied and build them to the street edge.
- » Public buildings that are infill projects should relate to adjacent buildings in terms of height, street wall definition, and proportion.
- » Provide shading devices along the building façade, particularly in areas with high pedestrian traffic.
- » Mixed-use buildings with an urban character that combine public uses along with other uses are encouraged.
- » If a public use is integrated into a larger mixed-use building, its façade treatment and layout should be integrated into the overall design, but it should still be clearly readable from the outside through signage, façade modulation, and the sizing and placement of the entrance.
- » Encourage shared lobbies, circulation spaces, courtyards, playgrounds, or garden areas in mixed-use buildings.



Pedestrian-Friendly Environment
Identity
Sustainability



Guidelines for Parking

Surface Parking

Parking Structures

Bicycle Parking and Facilities

Surface Parking

Reduce the visual impact of surface parking while providing sufficient parking space. Encourage shared parking, carsharing, bicycling, and reduced parking near transit.



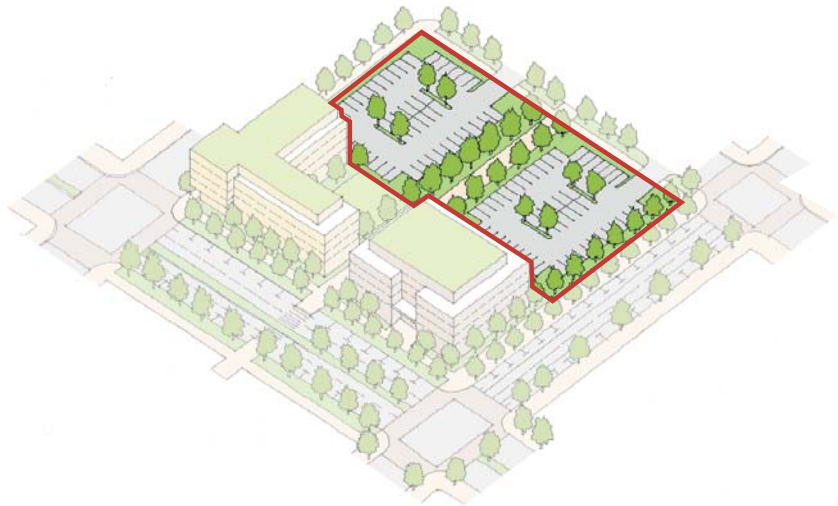
Permeable paving increases infiltration, reducing the amount of stormwater run-off.



Consider permeable materials such as decomposed granite for larger lots.

Discussion

Providing large amounts of traditional surface parking consumes large amounts of land, encourages driving, increases stormwater run-off, and is a major contributor to the urban “heat island” effect. This can create a hostile outdoor environment and raise building energy consumption, particularly in the summer months. The negative impacts of surface parking can be reduced by planting trees throughout lots, placing lots towards the northern edges of sites, encouraging the use of permeable paving, and prioritizing bicycles, motorcycles, and carshare and carpool vehicles. These site modifications bring the focus back to pedestrians and increase the overall sustainability of a development.



Place surface parking toward the side and rear of buildings; when surface parking is adjacent to a street edge, screen parking with trees and other landscaping,



Pedestrian-Friendly Environment
Identity
Sustainability

Guidelines

- » Locate surface parking lots along the side and/or rear of buildings, away from street edges; provide screening with appropriate landscaping in the perimeter setback.
- » Do not place large surface parking lots along North First Street in the Core Area, but permit a limited amount of short-term surface visitor parking between the street and building entrance.
- » Provide a generous amount of designated motorcycle and sheltered bicycle parking stalls (also see *Bicycle Parking and Facilities*, this section); place these stalls in preferential locations closest to building entrances and street edges.
- » Include stalls for carshare and carpool vehicles, and stalls specifically designed for small and compact cars; locate these stalls in preferential locations closest to building entrances.
- » Provide designated pedestrian pathways through parking lots that exceed 150 feet in length on one side.
- » Use water-permeable pavers or pavement to reduce stormwater runoff. Permeable pavement can also be used for parallel parking along private streets.
- » Use the Zoning Code's reduced parking requirements for projects within 2,000 feet of a light rail station.
- » Provide increased shading through increased tree plantings or solar-panel canopies to reduce the heat island effect.
- » Encourage shared parking facilities that take advantage of time differences in peak parking needs - for example, daytime office parking shared with evening retail or entertainment parking.
- » Encourage shared driveways or alleyways for parking access in order to reduce curb cuts and potential pedestrian/vehicle conflicts.
- » Limit the use of surface parking for residential developments.
- » On sites with structured parking and retail uses, provide a minimal amount of surface parking to meet the needs of retail establishments.
- » Privately-owned streets and driveways should be planned and designed to be similar to public streets, with curbs, trees, and parallel parking along both sides.



Closely-planted, large-canopied trees help reduce the urban heat island effect.



Solar panels utilized in parking lots can both generate electricity and provide shading for cars.



Screen the edges of parking lots that face onto a street with trees and other plantings.

Parking Structures

Encourage the use of parking structures rather than surface parking; reduce their visual and physical impact through smart placement and design.



Integrate parking structure entrances into the building design to conceal them.



Wrap structures with street-level retail to maintain a pedestrian-oriented environment.

Discussion

The use of structured parking can help increase densities and create a more urban environment while still supporting car use. To be successful, parking garages should not dominate the block. Garages that face directly onto streets create an unpleasant pedestrian experience, and fumes from garages can be very unhealthy for passing pedestrians. By screening garages through creative architectural design and landscaping, wrapping them with habitable space, placing them towards the center of blocks or underground, and utilizing them as sites for public art, parking garages can be integrated into pedestrian-focused places.

Guidelines

- » Locate parking structures at the side and/or rear of buildings, away from the street edge; or, provide a high-quality, multi-layered architectural façade that integrates the parking structure into its adjacent street frontage.
- » Integrate parking garage entrances into the building façade or garage façade design.
- » Except for underground garages, parking structures should not be located along North First Street.
- » Where provided, retail space that faces onto a primary street should meet the 45-foot minimum depth requirement and 15-foot floor-to-floor minimum height requirement specified in Guidelines for Buildings: Building Design and Materials. Deeper and taller dimensions, such as 60-foot depths or 18-foot floor-to-floor heights, are encouraged.
- » Where provided, leased spaces along a parking structure edge which are not on a primary street, should be at least 30 feet deep, and are anticipated to be service or office space rather than primary retail space.
- » In residential developments, structured parking that faces onto streets, open spaces, paseos, or pathways should be wrapped with habitable space whenever possible.
- » If not wrapped with habitable space, the parking garage floor should be placed no less than 4 feet below the grade of the adjacent sidewalk, and the above-grade portion should be well-



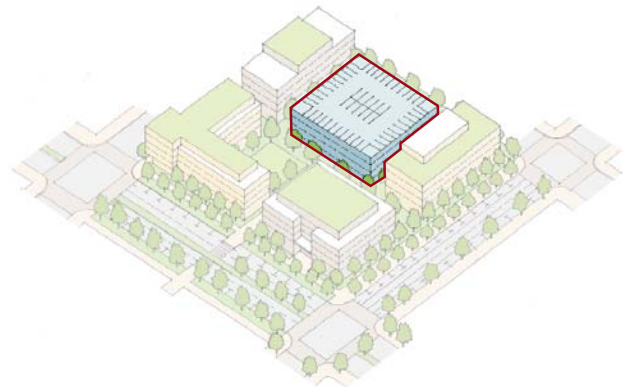
Pedestrian-Friendly Environment
Mix of Uses

Public Art
Sustainability

Guidelines (continued)

screened by architectural elements such as stoops, entryways, planters, or other features that are integrated into the overall building design.

- » Any exposed parking structure façade that faces neighboring residential development should be screened through architectural elements and/or landscaping.
- » Provide a generous amount of designated motorcycle and bicycle parking stalls on the ground level, closest to building entrances and street edges (see *Bicycle Parking and Facilities*, this section).
- » Locate designated stalls for carshare and carpool cars closest to building entrances.
- » Exposed parking structure openings should be screened with appropriate materials to minimize light emission at night.
- » Encourage the incorporation of public art in parking structures, particularly into building façades and wayfinding systems.
- » Encourage shared parking facilities that take advantage of time differences in peak parking needs - for example, daytime office parking shared with evening entertainment parking.
- » Use the Zoning Code's reduced parking requirements for within 2,000 feet of a light rail station.
- » Encourage green roofs to reduce the urban heat island effect.



Place parking garages towards the center of the block and screen with architectural details and/or plantings whenever adjacent to a street or pathway edge.



Encourage the screening of parking garages with high-quality, multi-layered façades.

Bicycle Parking and Facilities

Provide adequate, sheltered, and secure bicycle parking to support and encourage biking.

Discussion

Increased usage of transportation modes such as bicycling is key to reducing reliance upon the automobile. People will start bicycling more only when bike usage is encouraged and supported along every step of the way, making the bicycling experience smooth, seamless, and as easy as, if not easier than, driving a car. Bike trails and routes are one part of the equation; another part is secure bicycle parking facilities, particularly at home and at work, but also at parks, retail areas, and anywhere else automobile parking is already provided. Providing accessible, secure, and protected bicycle parking is a crucial step towards making bicycling a viable transportation option.



Provide abundant, easy-to-use bike parking in retail areas and near public facilities.



Well-designed, secure bicycle shelters can be integrated into common-use courtyards and can add to the design of a development.



Pedestrian-Friendly Environment
Connectivity

Identity
Sustainability

Guidelines

- » Provide adequate and easily-accessed bicycle parking in buildings, in open spaces, and along streets and shared pathways.
- » Ensure that bicycle parking facilities are highly visible and easy to find through clear signage. Utilize public art to reinforce visibility and relationship to its location.
- » Place bicycle parking in preferential locations closest to street edges and building entrances, especially retail entrances.
- » For outdoor facilities, prefer systems that include shelters and secure bike racks or lockers.
- » In areas with high usage, such as large campuses, consider centralized, enclosed, and managed bike parking facilities.
- » Include shower and changing facilities as applicable through the City's Zoning Code.



Provide clear signage so that bicycle facilities are easy to understand and use.



Large campus developments can provide secure, sheltered bicycle parking in centralized facilities.



Lockable bicycle parking structures can increase security.



Guidelines for the Streetscape

Street Hierarchy and Typologies

Street Hierarchy and Typologies: Typical Street Sections

Street Hierarchy and Typologies: Paseos

Street Hierarchy and Typologies: Parkways

Streetscape Design

Street Hierarchy and Typologies

Utilize a coherent street hierarchy and a set of typologies that includes high-quality streetscapes which accommodate and balance the needs of all transportation modes.

Discussion

A clear hierarchy of streets will help provide pedestrians, bicyclists, and motorists multiple ways in which to get to and through North San José. Such a system may be further defined through a set of street typologies that creates families of streets with common characteristics, such as sidewalk and street width, trees and landscaping, street furniture, setbacks, signage, and public art. Such a system will help make North San José recognizable as a district to residents and visitors, and contribute to North San José's identity.





Pedestrian-Friendly Environment
Connectivity
Identity

Guidelines

- » The street typologies on the following pages address the following characteristics:
 - » sidewalks, bike lanes, vehicular lanes, and street width;
 - » tree and landscape strategy;
 - » pedestrian amenities; and
 - » setbacks and adjacent uses.
- » Cross-sections are shown for the following streets:
 - » North First Street:
 - » North First Street - South, from I-880 to US-101
 - » North First Street - Core Area, from US-101 to Tasman Drive
 - » North First Street - North, from Tasman Drive to SR-237
 - » Zanker Road:
 - » Zanker Road - South, from US-101 to Montague Expressway
 - » Zanker Road - North, from Montague Expressway to Tasman Drive
 - » Mixed-Use Retail Streets - with parallel or diagonal parking
 - » Industrial/Grid Streets
 - » Residential Streets - with and without amenity retail
 - » Paseos
 - » Parkways - with and without on-street parking

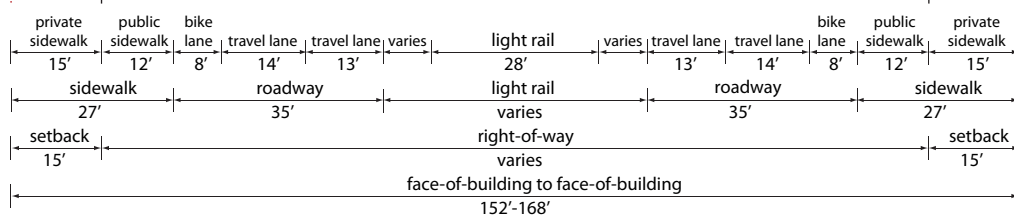
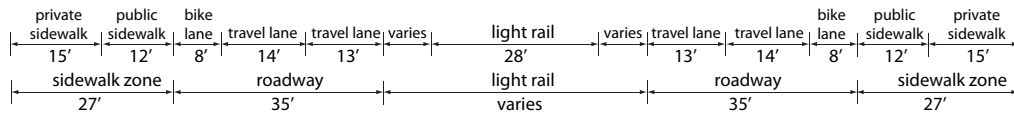
Note: All cross-sections must be verified with the City.



A well-designed street considers and balances the needs of multiple modes of movement.

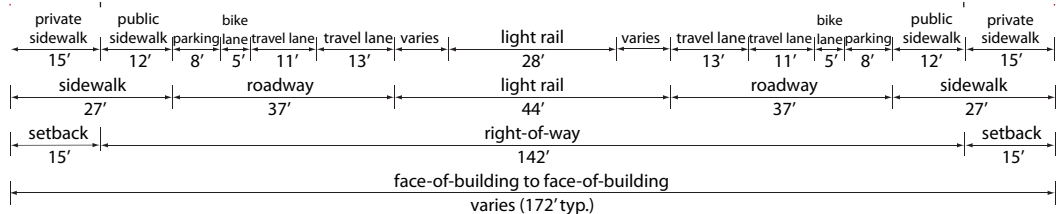
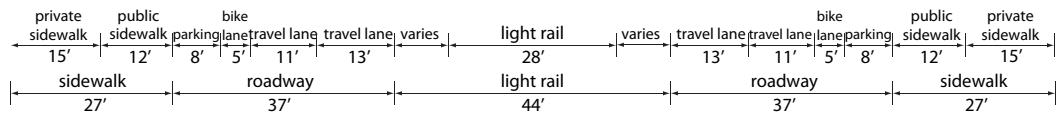
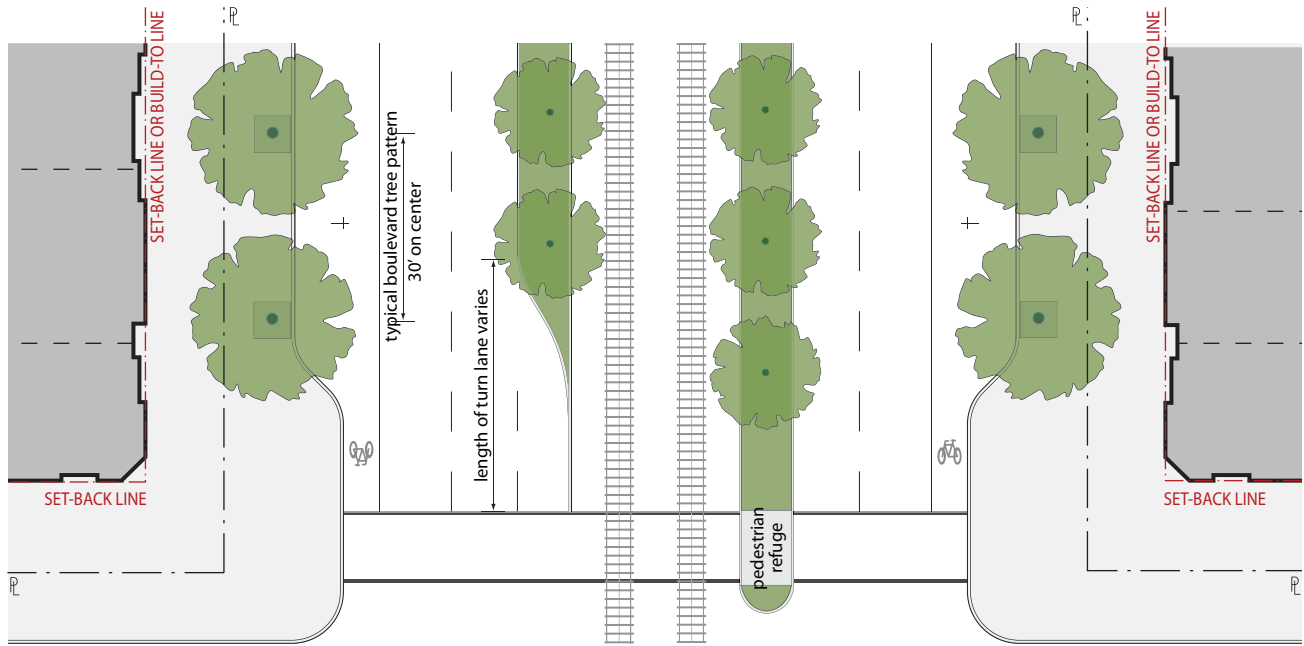
Street Hierarchy and Typologies: Typical Street Sections

1a North First Street: South - I-880 to US-101



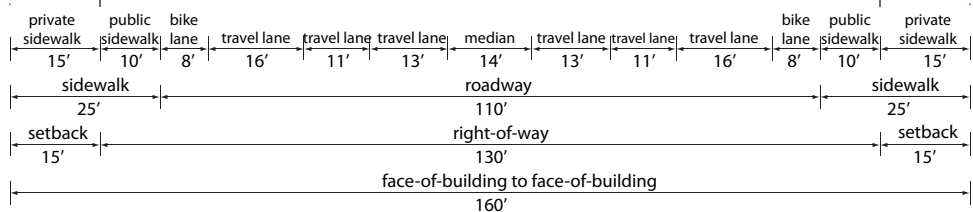
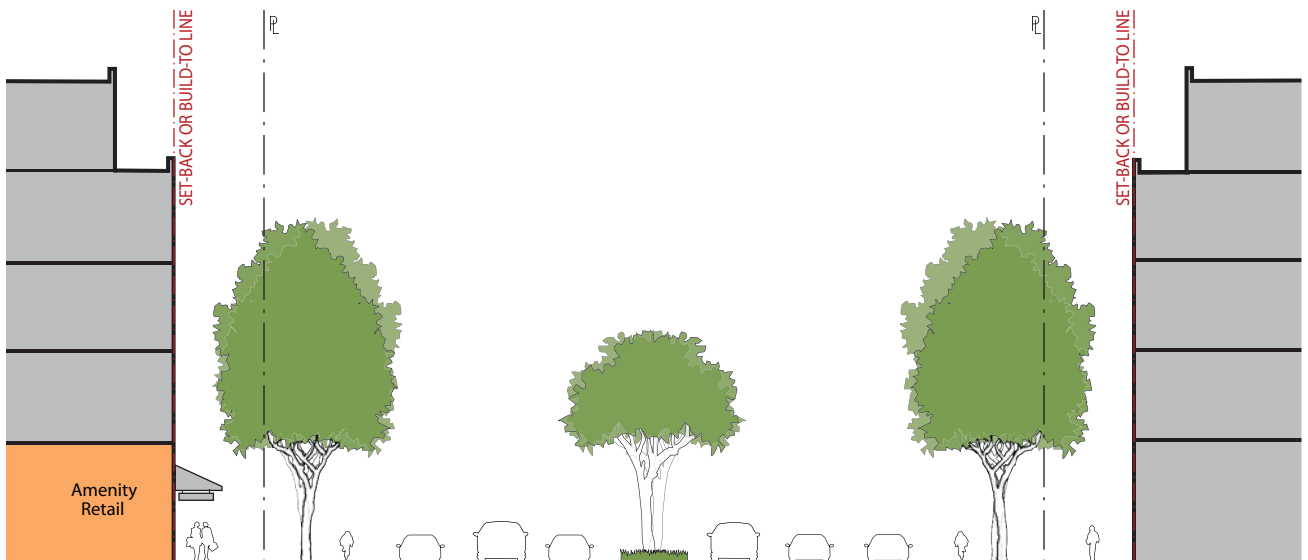
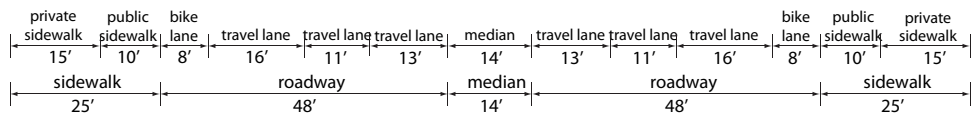
NOTE: For details about the building face as related to the street, please refer to *Site Planning Guidelines: Street Frontages*.

1b North First Street: Core Area - US-101 to Tasman Drive



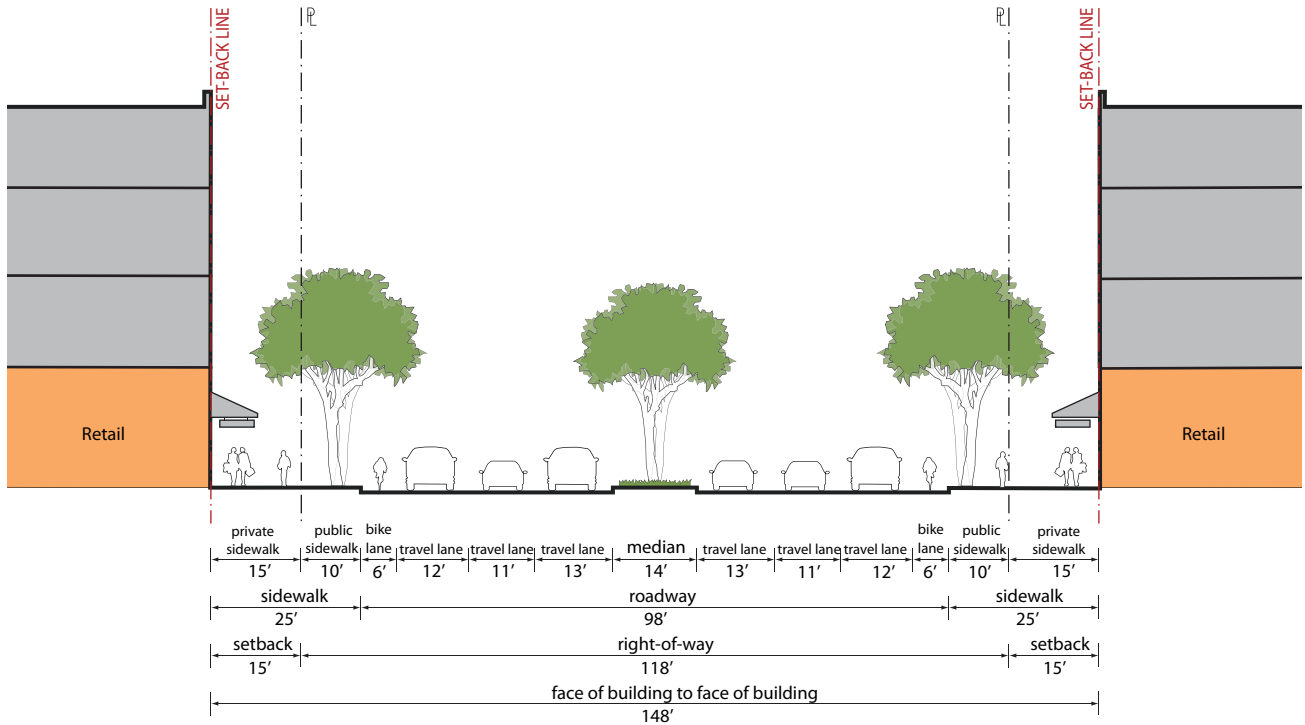
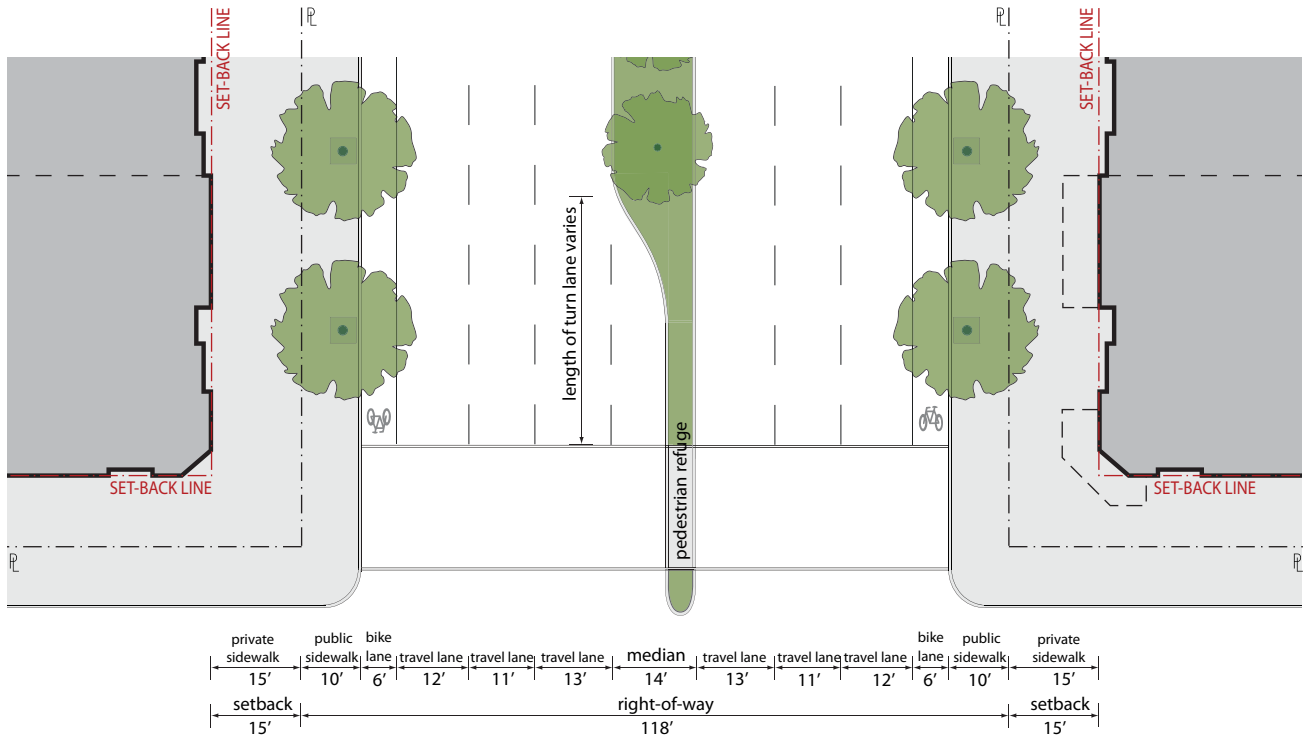
NOTE: For details about the building face as related to the street, please refer to *Site Planning Guidelines: Street Frontages*.

1c North First Street: North - Tasman Drive to SR-237

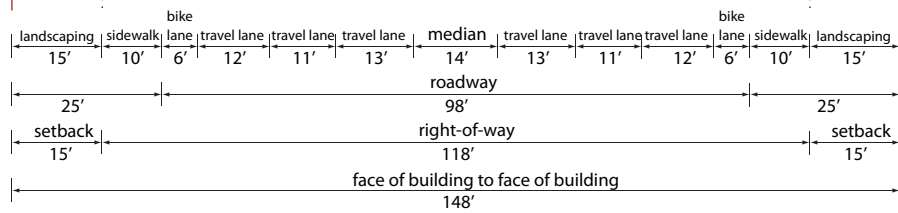
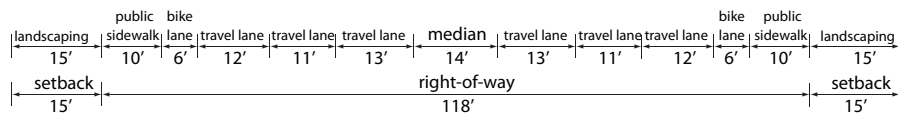
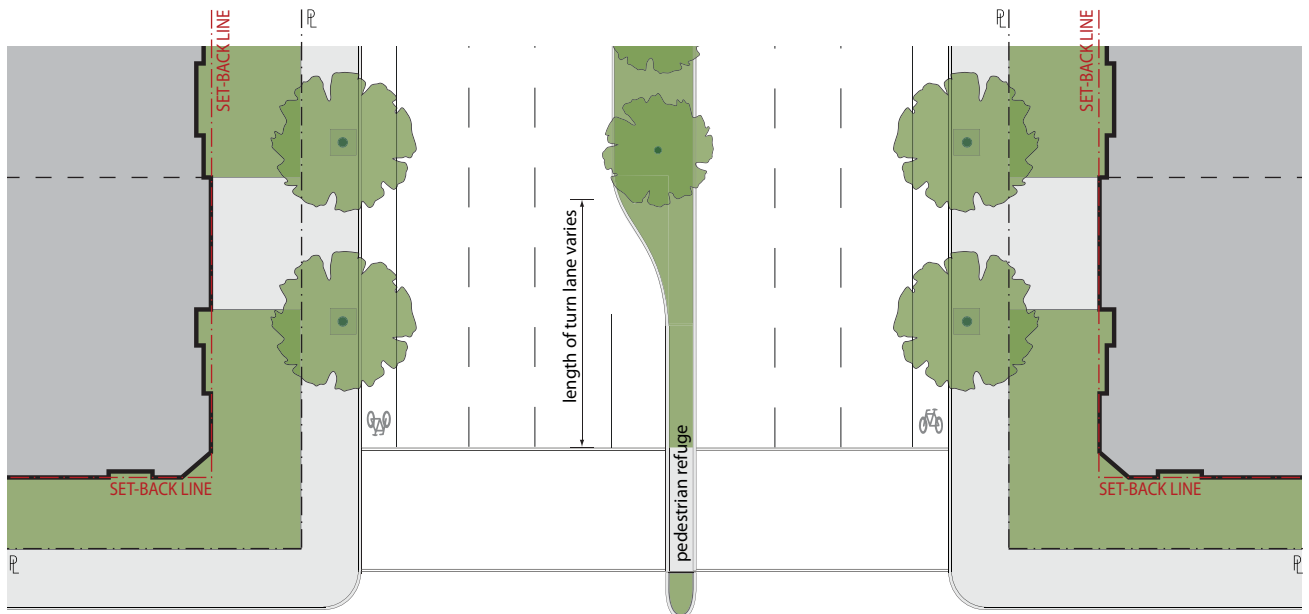


NOTE: For details about the building face as related to the street, please refer to *Site Planning Guidelines: Street Frontages*.

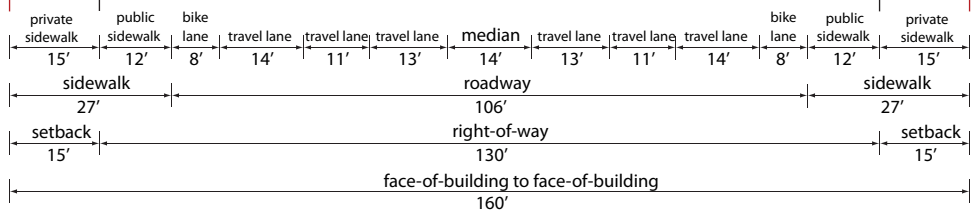
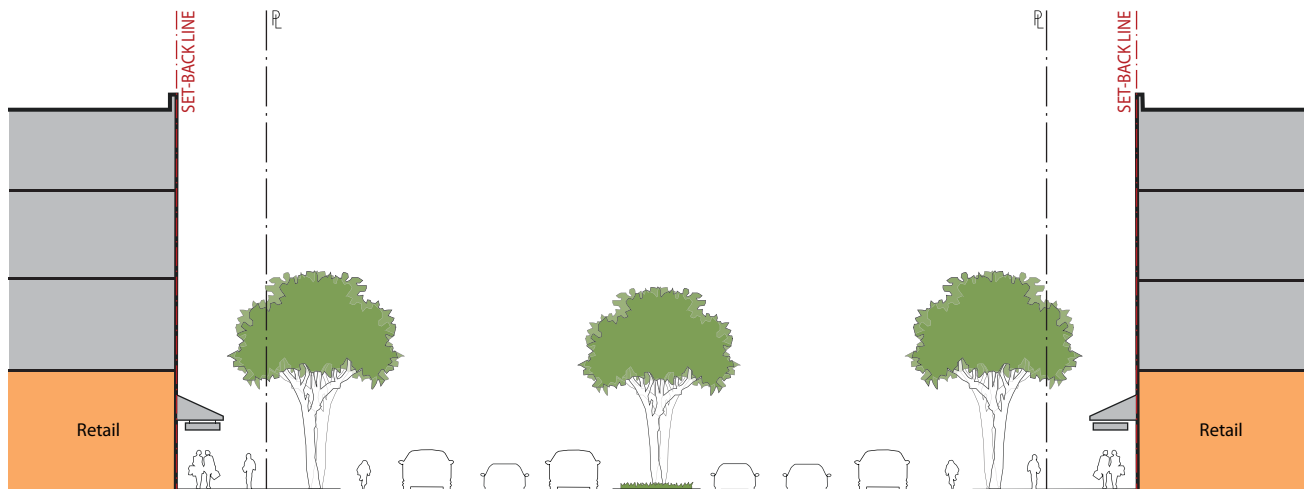
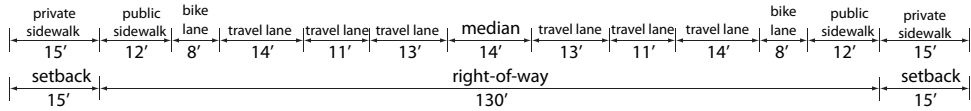
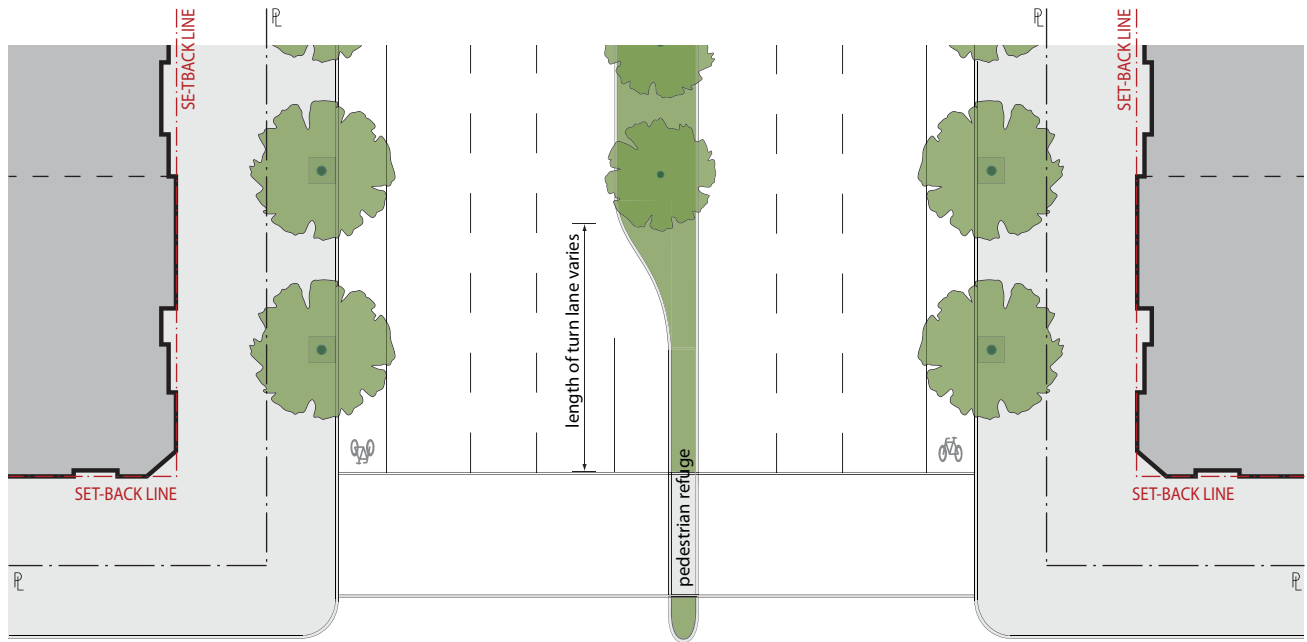
2a Zanker Road: South - US-101 to Montague Expressway with Retail



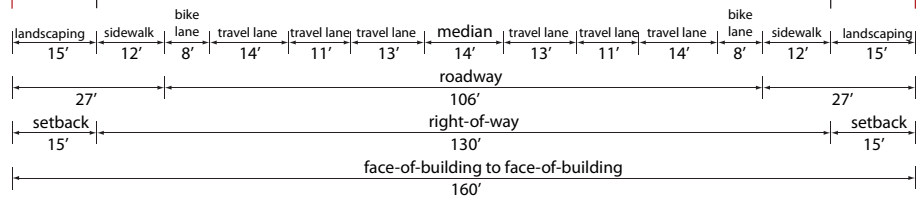
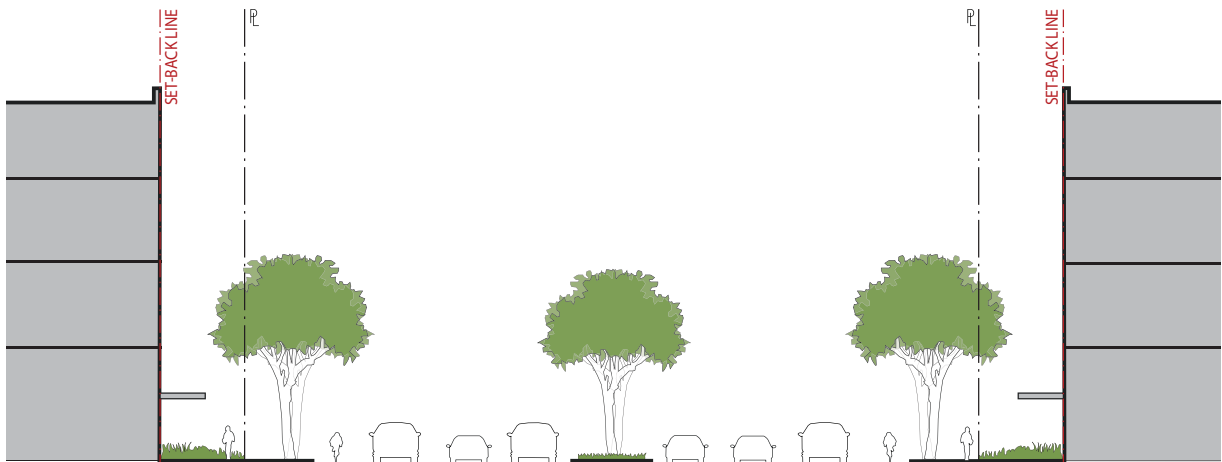
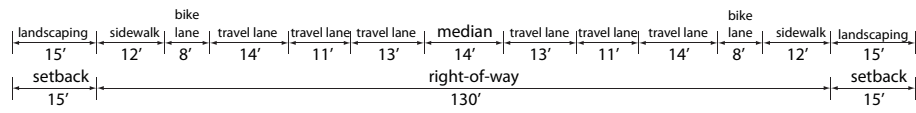
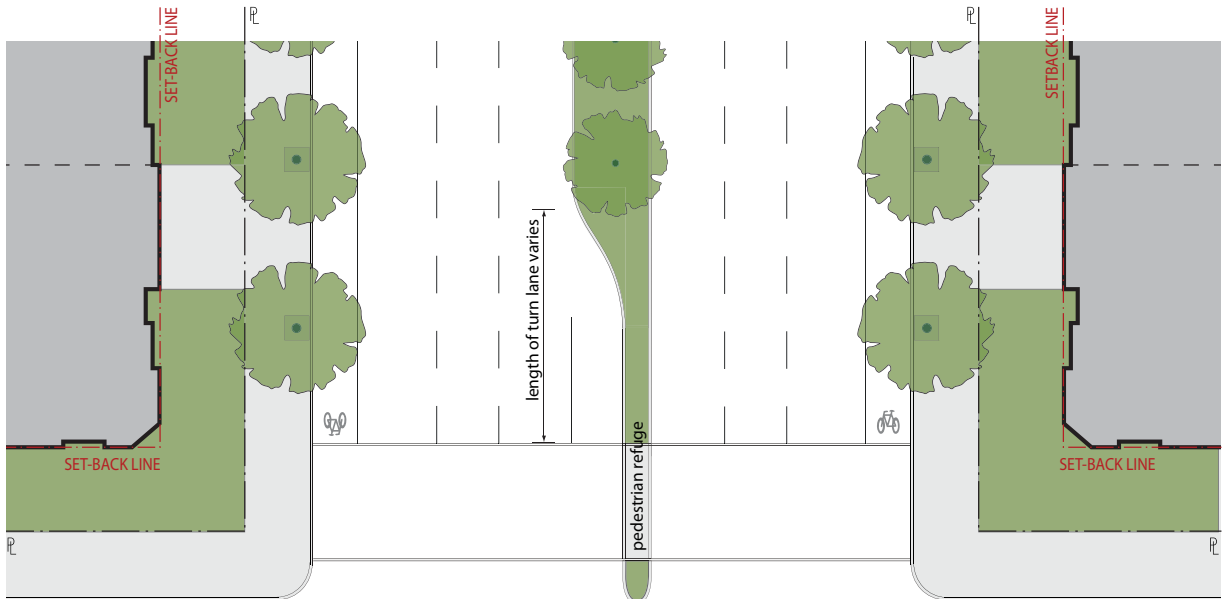
2b Zanker Road: South - US-101 to Montague Expressway without Retail



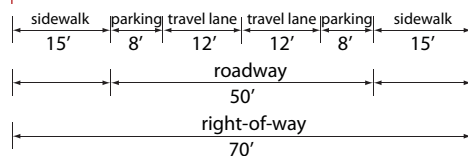
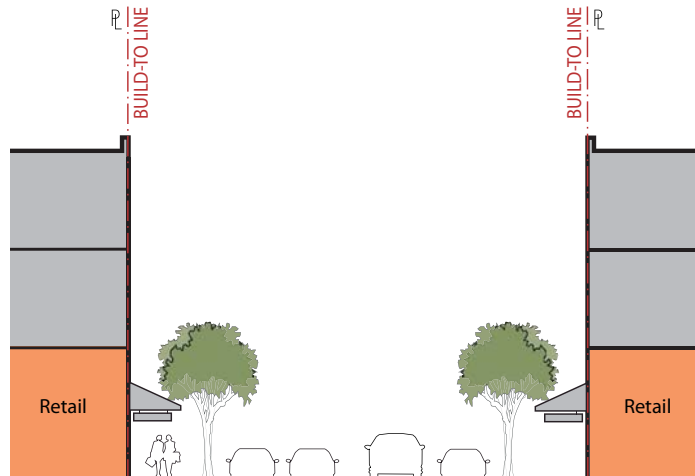
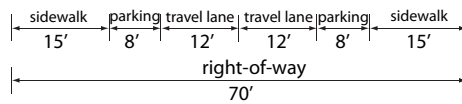
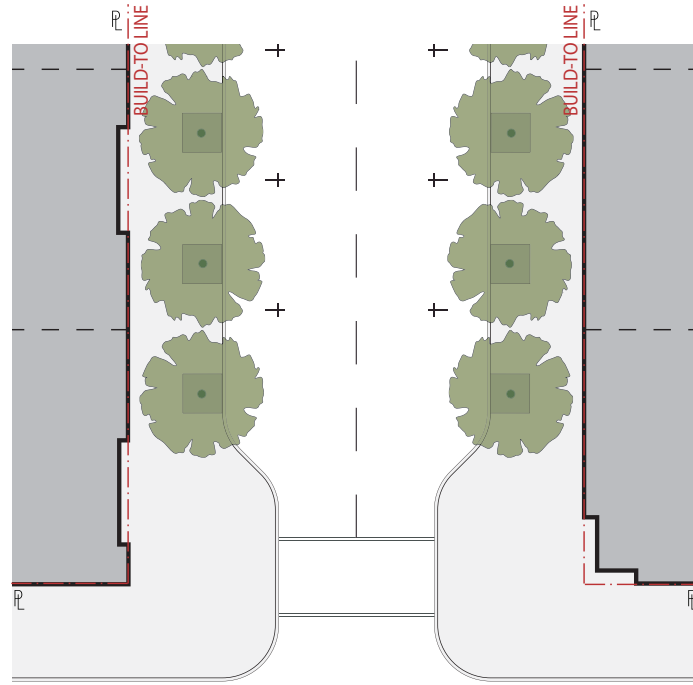
2c Zanker Road: North - Montague Expressway to Tasman Drive with Retail



2d Zanker Road: North - Montague Expressway to Tasman Drive without Retail

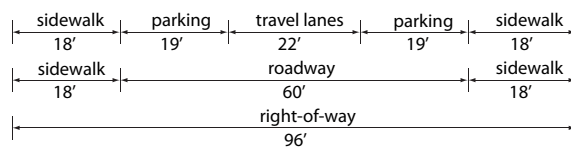
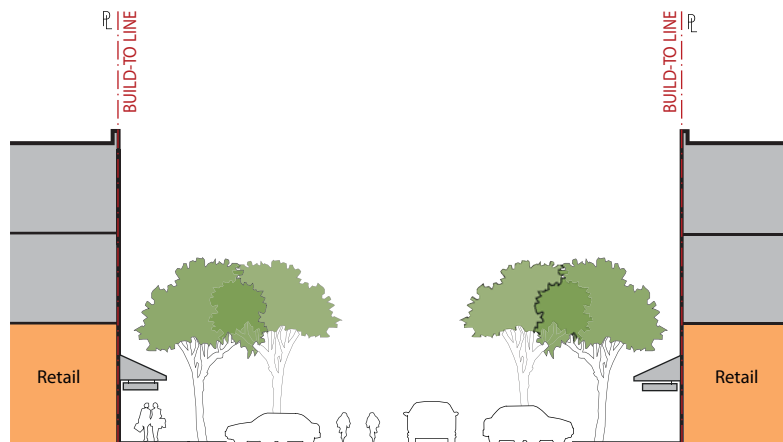
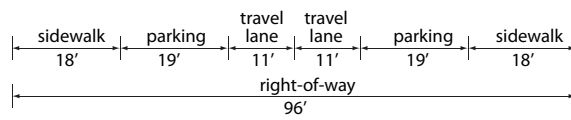
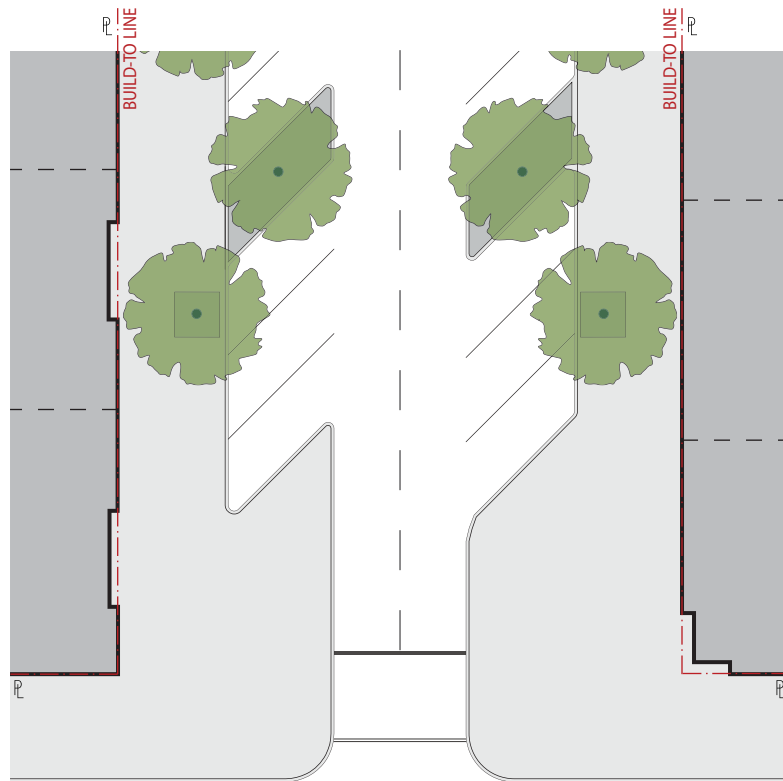


3a Mixed-Use Retail Street with parallel on-street parking



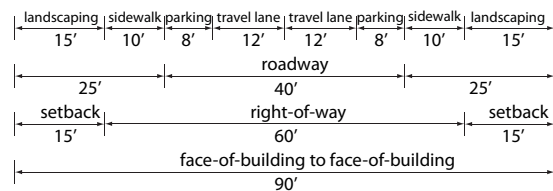
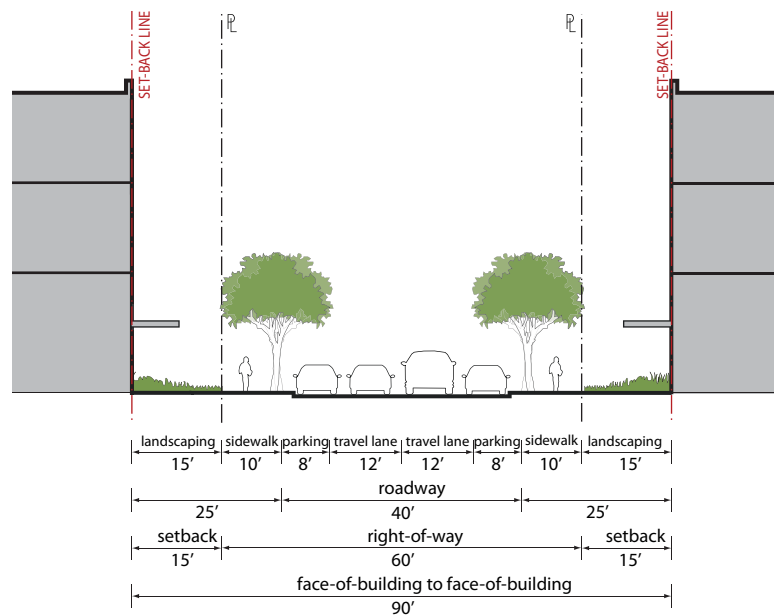
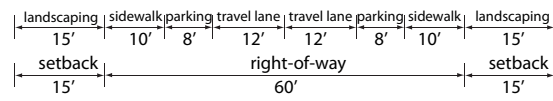
NOTE: For details about the building face as related to the street, please refer to *Site Planning Guidelines: Street Frontages*.

3b Mixed-Use Retail Street with diagonal on-street parking



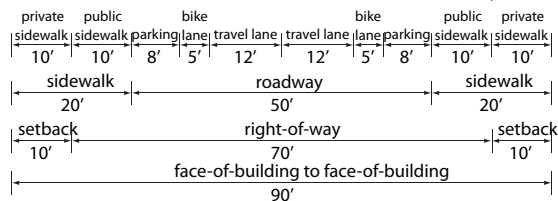
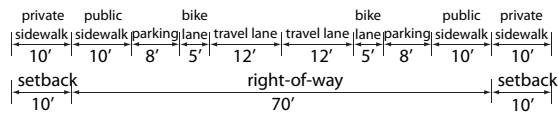
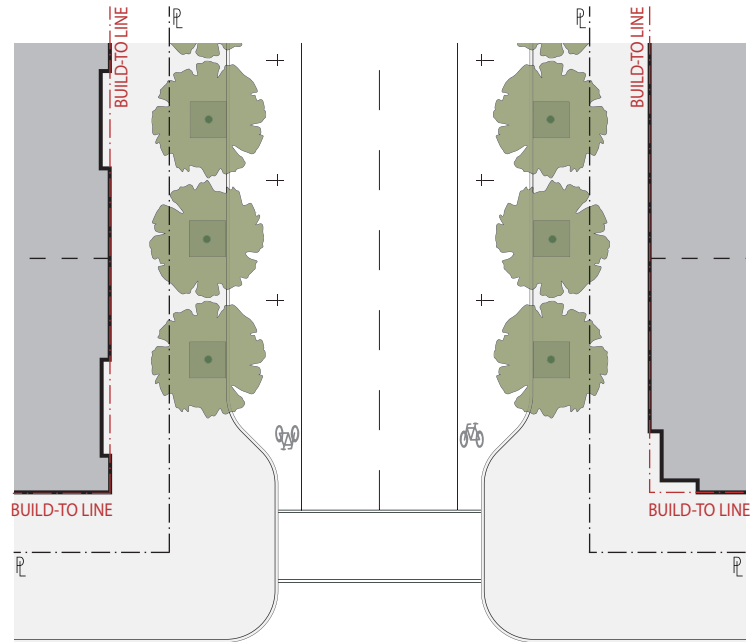
NOTE: For details about the building face as related to the street, please refer to *Site Planning Guidelines: Street Frontages*.

4 Industrial/Grid Street



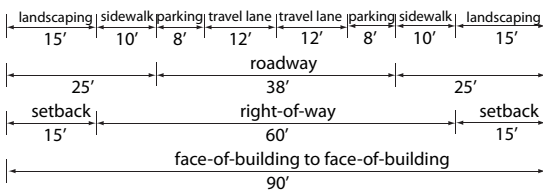
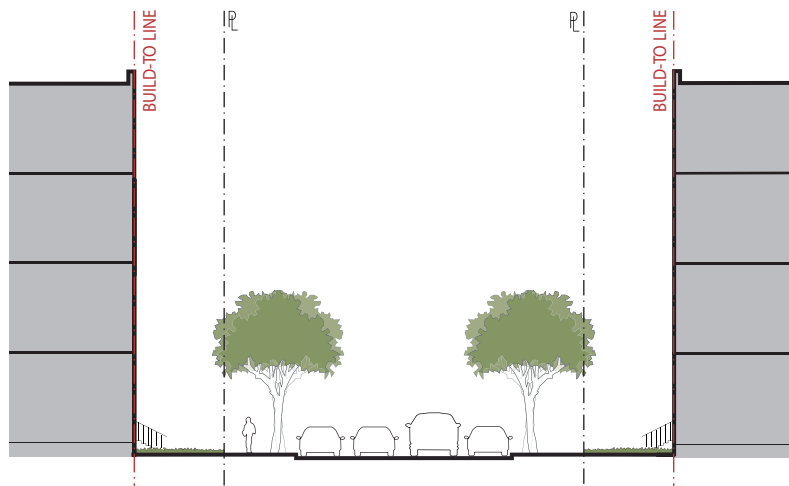
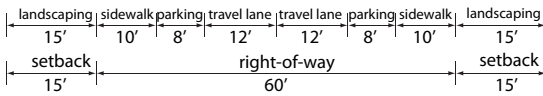
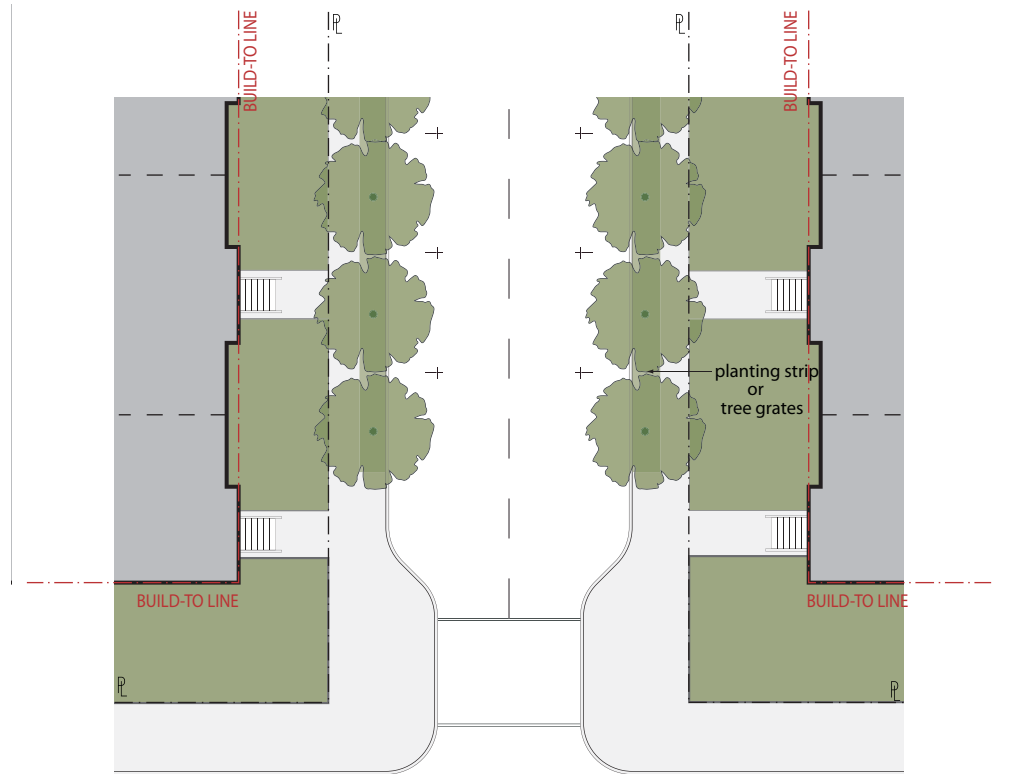
NOTE: For details about the building face as related to the street, please refer to *Site Planning Guidelines: Street Frontages*.

5a Residential Street with Amenity Retail



NOTE: For details about the building face as related to the street, please refer to *Site Planning Guidelines: Street Frontages*.

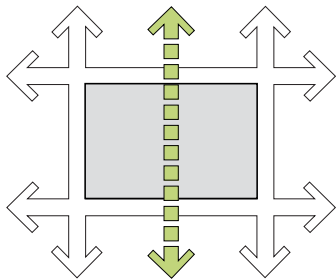
5b Residential Street without Amenity Retail



NOTE: For details about the building face as related to the street, please refer to *Site Planning Guidelines: Street Frontages*.

Street Hierarchy and Typologies: Paseos

Provide public or publicly-accessible, interconnected paseos – combined pedestrian/bicycle-only paths – when streets are not feasible.



Mid-block connections like paseos offer useful shortcuts to pedestrians and bicyclists.



Paseos can offer a more comfortable pedestrian experience, particularly in summer.

Discussion

Paseos provide shortcuts that encourage walking and biking by increasing visibility and accessibility between different areas of North San José. They can also provide a more shaded experience, increasing pedestrian comfort, particularly in the hot summer months.

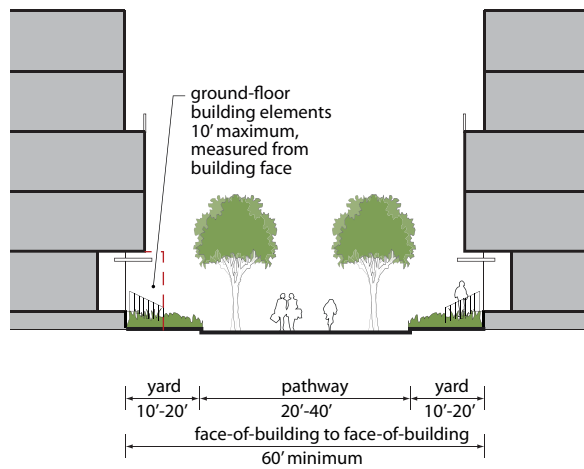
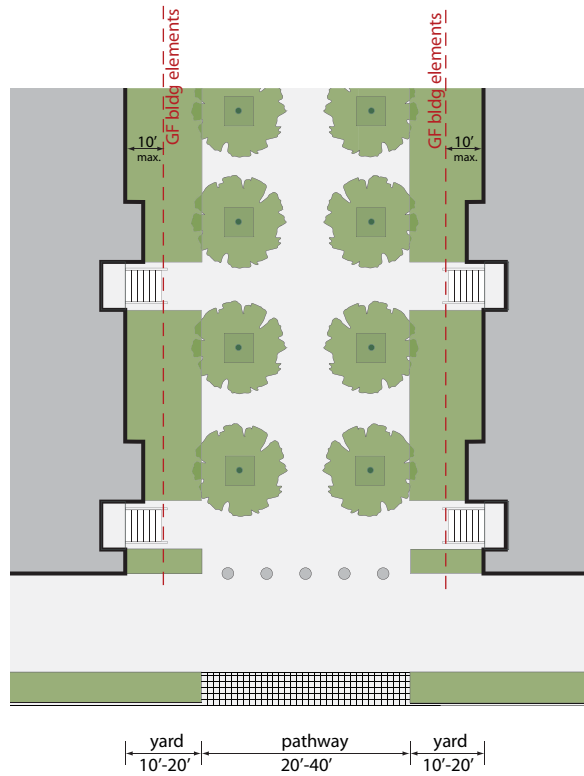
Guidelines

- » Paseos may be used to divide residential blocks and shall satisfy these Guidelines. They shall be open to the public.
- » When streets are not feasible, provide paseos for public circulation. When provided in lieu of a new street, paseos should have the same width as a comparable street.
- » Connect paseos to pedestrian pathways and public streets, plazas and open spaces; interconnect paseos to form a network.
- » Paseos must not be gated and must be kept publicly-accessible. Encourage creative ideas in partnership with the private sector to address security needs while increasing walkability.
- » Paseos should have a minimum width of 60 feet from building face to building face. Ground-floor building elements that project a maximum of 10 feet from the building face are allowed.
- » Visibility should be maintained through each paseo from one end to the other.
- » Provide a 20-foot-wide clear pathway if a fire lane is required.
- » Provide trees, landscaping, street furniture, and pedestrian lighting to create a street environment.
- » Develop a consistent palette of street furniture and materials in one area to make the paths recognizable as an interconnected network.
- » Prefer water-permeable surfaces. If on-site water retention is intended, encourage the integration of stormwater collection systems such as bioswales and rills.
- » Integrate public art as a part of provided amenities and as unique elements to enhance the pedestrian experience. Encourage building owners to incorporate artist-designed elements into façades to create a more unique and identifiable presence.



Walkability
Pedestrian-Friendly Environment
Connectivity

6 Paseo



Street Hierarchy and Typologies: Parkways

Provide parkways to enhance pedestrian and bicycle connections, particularly between the Guadalupe River and Coyote Creek.

Discussion

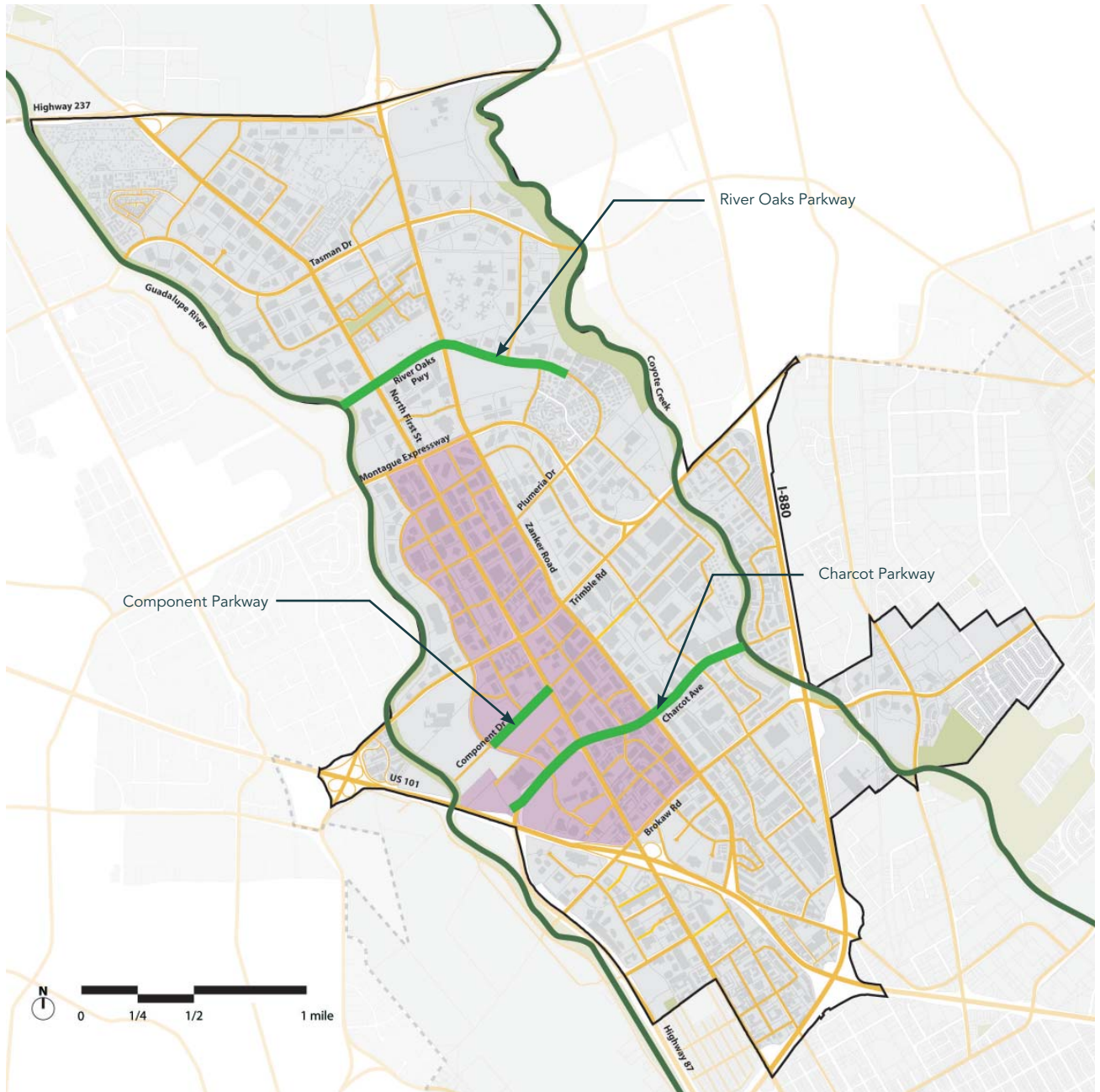
Parkways are streets that have a more lush, vegetated character created through a combination of planted medians, generous landscaping along their street and building edges, and two lanes of traffic moving at relatively slow speeds. Parkways are an ideal way to provide enhanced pedestrian and bicyclist connections between the Guadalupe River and Coyote Creek, thus connecting the street network to the trail network. The parkways serving this purpose in North San José are River Oaks Parkway, Charcot Parkway, and Component Parkway.

Guidelines

- » Parkways should include a planted median, bike lanes in both directions, sidewalks of a minimum width of 12 feet, and a double row of trees.
- » The setback areas of buildings that face onto parkways should include more lush informal landscaping.

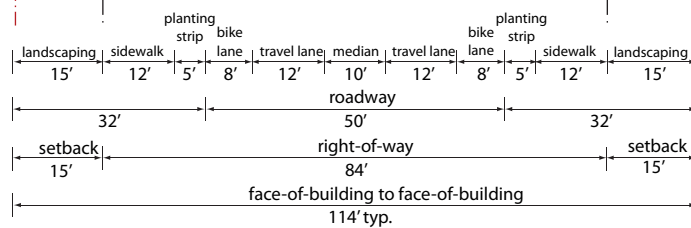
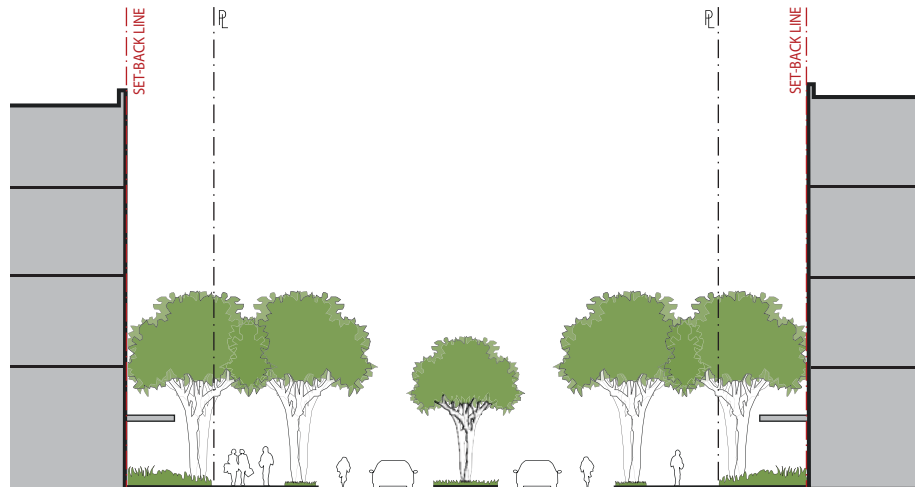
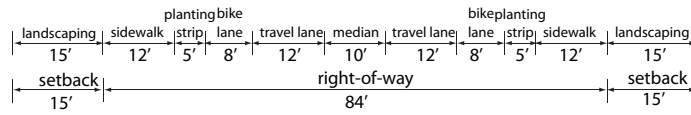
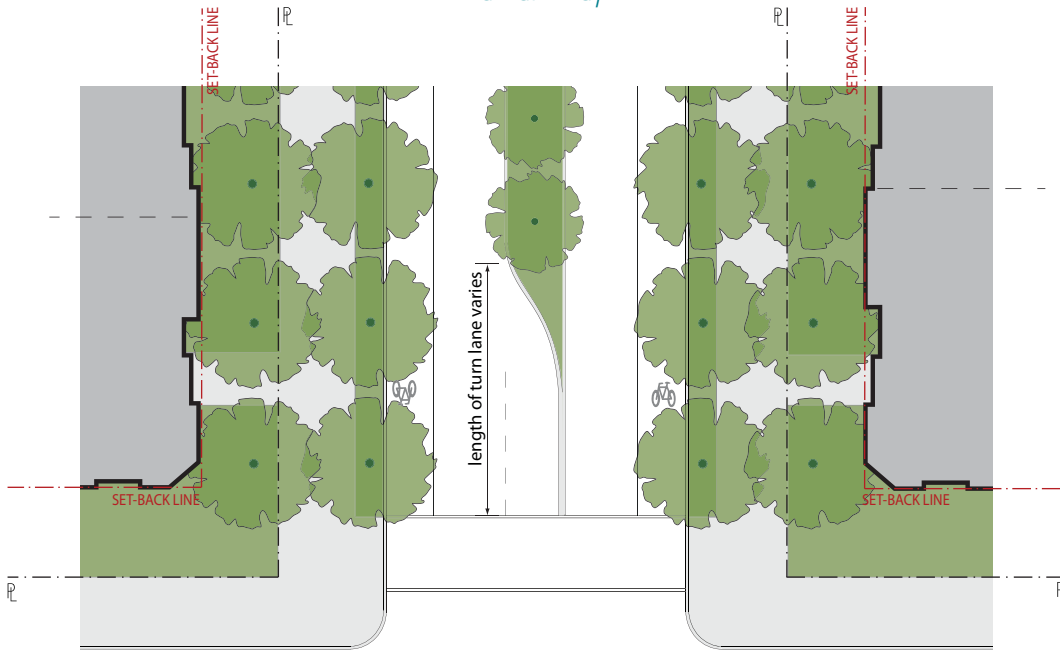


Parkways with a generous sidewalk and a double row of trees can create a verdant connection between trails.

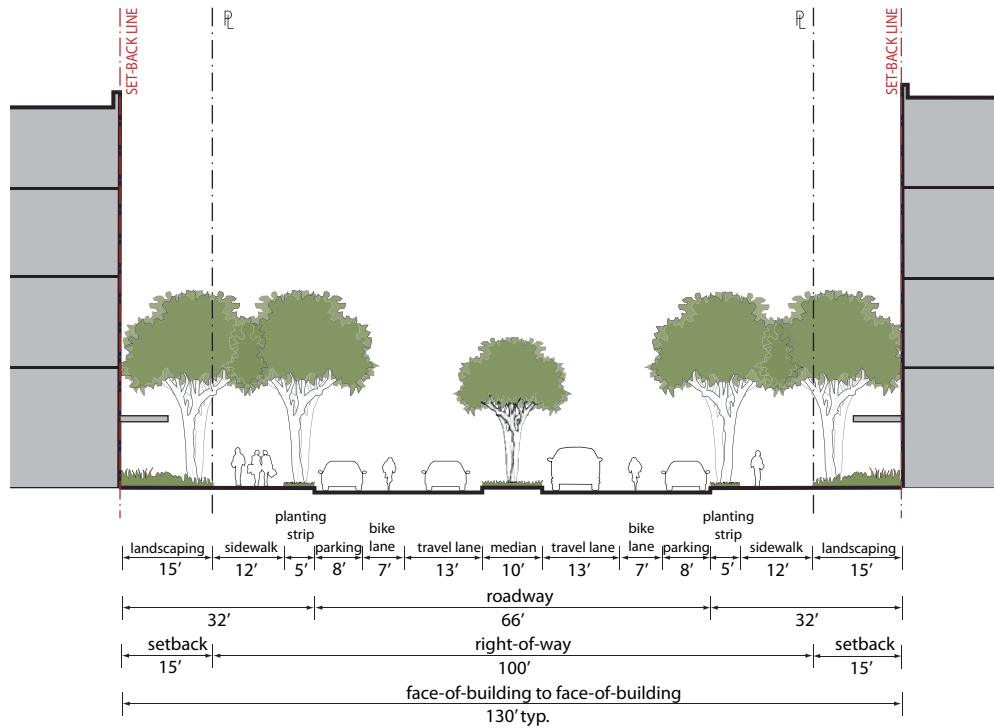
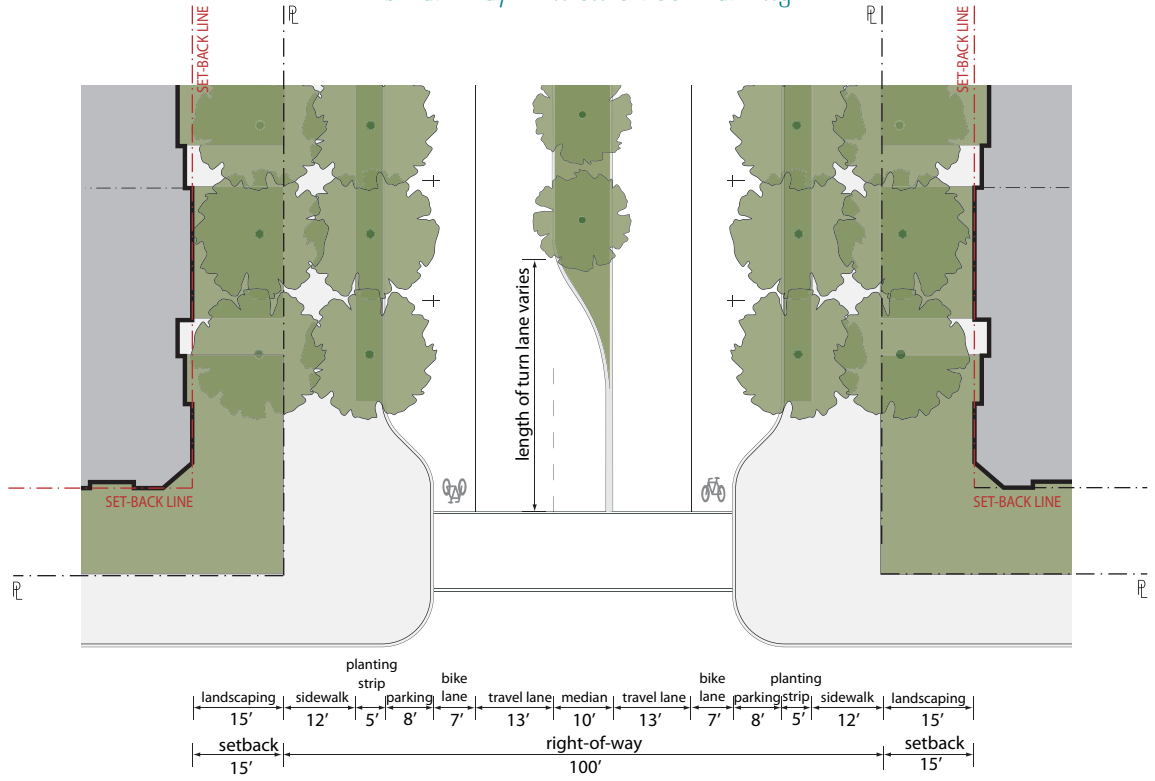


Parkways serving as enhanced east-west pedestrian/bicyclist connections in North San José.

7a Parkway

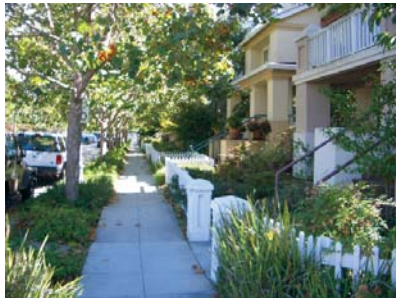


7b Parkway with On-Street Parking



Streetscape Design

Develop and use a palette of streetscape design elements to enhance the pedestrian environment.



Sidewalks in residential neighborhoods can have a green character.



Street furniture such as planters and benches along a sidewalk help soften the street edge, creating a comfortable buffer between cars and pedestrians.

Discussion

Streetscape elements can help support and guide people on their way through North San José. Features such as benches, flower planters, bike racks, lighting, public art, signage, and drinking fountains enhance sidewalk areas and provide needed amenities to pedestrians while they're visiting a neighborhood. Pedestrian bulb-outs, mid-crosswalk refuges, and crosswalk pavement changes help make streets with heavy traffic feel more pedestrian-friendly, encouraging walking and transit use.

Guidelines

Pedestrian Crossings

- » Use features such as bulb-outs and changes in pavement to improve visibility and pedestrian comfort.
- » On wider arterial streets, include mid-crosswalk pedestrian refuges.

Street Furniture

- » Along North First Street, use a signature palette of street furniture and lighting to help define North San José's identity.
- » Integrate public art into planned amenities to create unique and engaging streetscapes.
- » Integrate bus shelters into overall streetscape design, placing them away from the street edge when possible.
- » In retail areas, utilize street furniture such as benches and planters to enhance the pedestrian realm and soften the street edge.

Sidewalks

- » In residential areas or along private streets, allow sidewalk areas to have a more vegetated character - along with planting strips, consider using bioswales for stormwater filtration (also refer to *Guidelines for the Streetscape: Street Hierarchy and Typologies* and *Guidelines for Site Planning: Street Frontages*).
- » Consider using permeable pavers for private sidewalks.



Pedestrian-Friendly Environment Public Art
Identity

Guidelines (continued)

Street Parking

- » In retail districts, encourage the use of parallel or diagonal on-street parking to provide parking for short-term visits (also refer to *Guidelines for the Streetscape: Street Hierarchy and Typologies*).
- » Consider creative paving options in parking areas; for example, mark parking spots through a change in pavement rather than through striping.

Traffic Calming

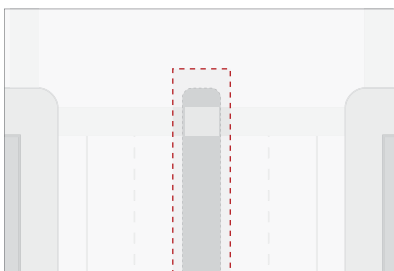
- » Particularly in residential areas, encourage the implementation of traffic calming measures such as speed humps, traffic circles, and chicanes.



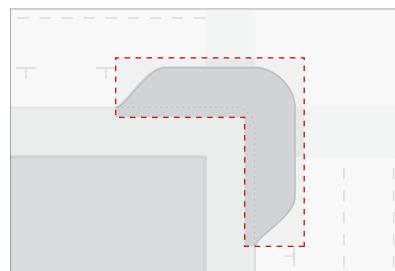
Mid-block crossings with pavement changes emphasize that the public realm is for pedestrians as well as for cars.



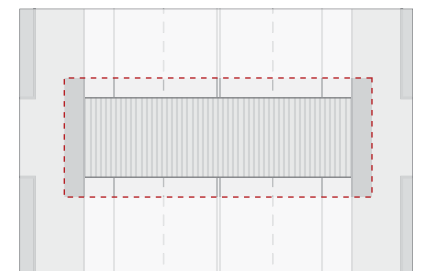
Bulb-outs, like this planted one, can help create a more pleasant pedestrian experience.



Pedestrian refuges make arterials easier and more pleasant to cross.



Bulb-outs reduce the apparent width of wide arterials.



Pavement changes for crosswalks increase the presence of the pedestrian realm.

Streetscape Design

Guidelines (continued)

Street Trees

- » Use a diverse palette of climate-appropriate, and when possible, native, trees throughout the North San José area.
- » Create a more unified street tree design along the most important streets - North First Street, Zanker Road, and Tasman Drive.
- » Allow for a more varied palette of trees within residential areas.

Street Tree List

- » Small - 3-4 foot-wide planting site
 - » *Lagerstroemia indica* - Crape Myrtle
 - » *Cercis canadensis* - Eastern Redbud
 - » *Tristaniaopsis laurina* - Swamp Myrtle
 - » *Acer buergerianum* - Trident Maple
- » Medium - 4-5 foot-wide planting site
 - » *Pyrus calleryana* 'New Bradford' - New Bradford Pear
 - » *Maytenus boaria* - Mayten Tree
 - » *Tipuana tipu* - Tipu Tree
 - » *Pittosporum undulatum* - Victorian Box
 - » *Carpinus betulus* 'Frans Fontaine' - Upright European Hornbeam
 - » *Magnolia grandiflora* 'St. Mary' - St. Mary Magnolia
- » Large - 5-6' foot-wide planting site
 - » *Tilia tomentosa* - Silver Linden
 - » *Zelkova serrata* - Sawleaf Zelkova
 - » *Acer x Freemanii* 'Autumn Blaze' - Autumn Blaze Maple
 - » *Quercus rubra* - Northern Red Oak
 - » *Quercus shumardii* - Southern Red Oak
 - » *Koelreuteria bipinnata* - Chinese Flame Tree
 - » *Betula nigra* 'Heritage' - River Birch
 - » *Quercus virginiana* - Southern Live Oak
 - » *Quercus suber* - Cork Oak



Pedestrian-Friendly Environment Public Art
Identity



Guidelines for Parks, Trails, and Open Space

Parks, Trails, and Open Space Elements

Parks Design

Neighborhood Parks

Community Park

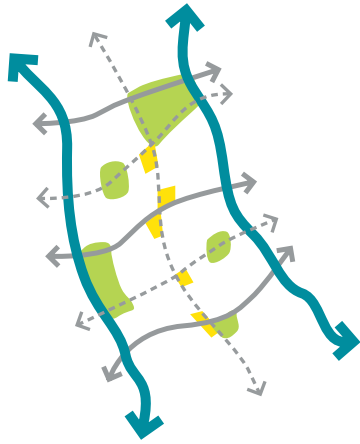
Central Urban Park/Plaza

Trails

Trail Design

Parks, Trails, and Open Space Elements

Ensure that all new public recreational areas are part of a larger open space network of parks, plazas, trails, and paseos in North San José and surrounding areas.



Physically connect public and private parks and plazas to trails, paseos, and pedestrian paths.

Discussion

An array of easily-accessible public and private open spaces of various sizes and characters is an essential component of a lively community. A good distribution of such spaces will provide recreational, educational, sporting, and cultural benefits to residents, visitors, and employees. Parks in North San José can both respond to the existing character and needs of a neighborhood while also serving as nodes that spur new development. By connecting parks, plazas, and other open spaces to the existing and planned street network with a consistent system of wayfinding signage and public art, a coherent and highly accessible network of publicly-accessible open spaces can be created.



Develop a large central urban plaza to provide flexible open space and unify the civic identity.



Provide multiple medium-sized parks and plazas and connect them via pathways and streets.

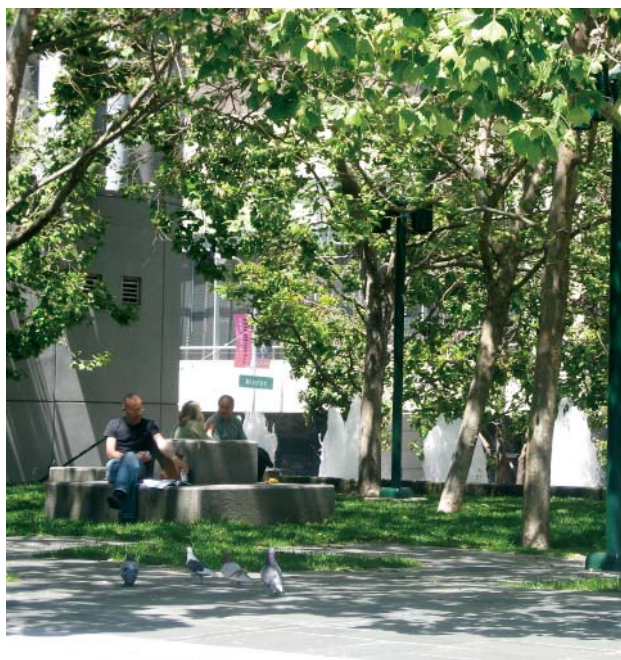


Walkability
Pedestrian-Friendly Environment
Connectivity

Identity
Public Art
Sustainability

Guidelines

- » Develop a variety of park and plaza designs that respond to and help distinguish the different zones in North San José.
 - » Provide one large community park or two smaller parks of equal total acreage that emphasize sporting and recreational activities.
 - » Provide smaller neighborhood parks in residential communities.
 - » In the Core Area, create a large centrally-located urban plaza with a civic identity as well as multiple evenly-distributed, medium-sized public plazas.
- » Make parks a focal point of development in each area. Encourage retail and/or residential uses around parks to activate them.
- » Visually and physically connect parks and plazas to trails, paseos, and pedestrian paths.
- » North San José parks should follow the character district palette now under development by the Department of Parks, Recreation and Neighborhood Services.
- » Integrate public art throughout the public open space network.
- » Utilize public art to encourage engagement with open space and exploration of trails.



Provide smaller parks and plazas with plenty of shading and seating along pedestrian paths within neighborhoods.

Parks Design

Ensure that parks reflect high-quality, sustainable design that serves the needs of North San José residents, employees, and visitors.

Discussion

A network of public parks, plazas and outdoor spaces can become a source of pride to the community. The use of creative strategies for the design, operation, and maintenance of public places will ensure that they become well-used and much-loved community assets. Walking paths through parks that include well-designed and attractive paving materials, lush landscaping and abundant shade, carefully designed lighting fixtures and lighting levels, high quality outdoor furniture such as benches, signs and fences, and unique public art, can all contribute to the operational uses of such spaces as well as to the overall perception of a desirable outdoor environment.



A carefully chosen palette of materials, in combination with good design, can create high-quality parks.



Use native plants and permeable surfaces like decomposed granite when possible.



Pedestrian-Friendly Environment
Identity

Public Art
Sustainability

Guidelines

- » Encourage creative design of park buildings, landscaping and built elements such as paths, fences, seating, playground equipment, lighting, signage, and water features.
- » Design parks with zones to respond to a variety of needs, such as shade, quiet areas, play areas, or group gatherings; create separate areas for active uses and informal uses.
- » Provide cross-connecting walking paths so that parks enhance pedestrian connections within the neighborhood.
- » Provide varied seating opportunities such as walls, benches, lawns and chairs, in both shaded and sunny, open and wind-protected locations. Place furniture so as to allow for impromptu gatherings and group seating.
- » Throughout the park system, incorporate public art that reinforces a sense of place and enhances engagement.
- » Provide immediate shade opportunities with shade structures and follow up with mature, large canopy trees.
- » Do not allow emergency vehicle access to private developments through public parks or open space.
- » Provide a distinctive character for park development in North San José by using the palette under development by the Department of Parks, Recreation and Neighborhood Services.
- » Prepare a park master plan for the design of each park site as part of the City's regular design process.



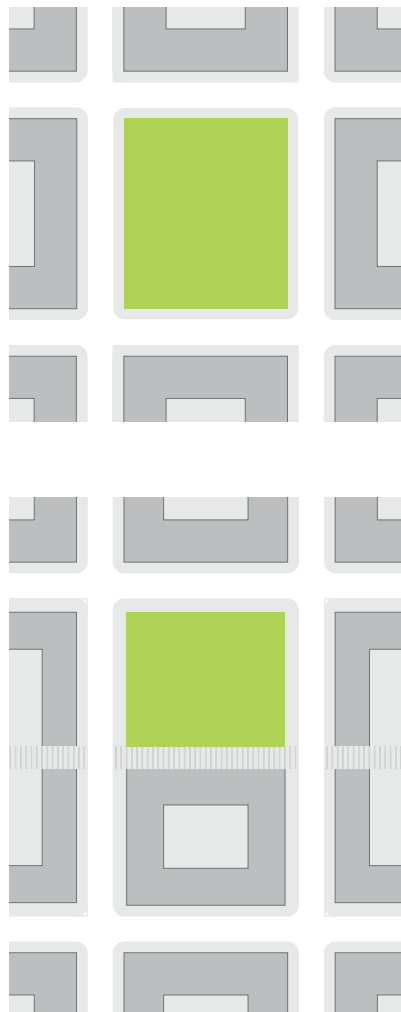
Cross-connecting paths can contribute added activity and also serve as shortcuts within the neighborhood.



Include abundant seating opportunities throughout parks.

Neighborhood Parks

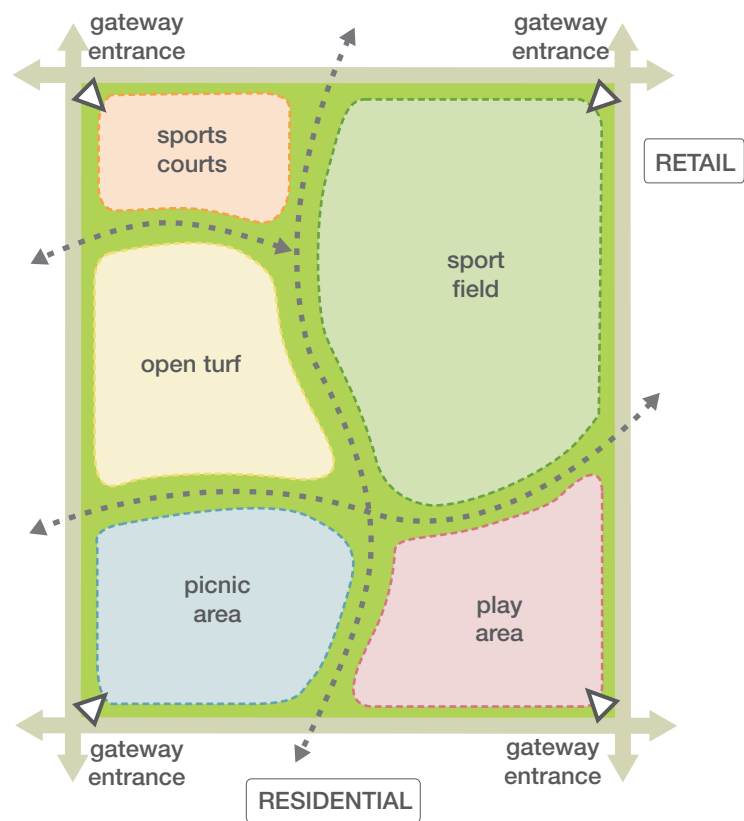
Create parks in new housing opportunity areas that are at least three acres in size and that are within 1,000 feet of residences.



Neighborhood parks should be laid out with public streets on at least two sides, and preferably on all four sides.

Discussion

New and existing residential developments which are provided with local park facilities will develop strong community links. A network of local parks that meet the needs of nearby residents of all ages and offer recreational and leisure space such as walking paths, lawns, and picnic facilities, will encourage daytime use and community interaction. Residents who perceive their local public parks to be a safe, secure, usable, and well-maintained places will embrace them and use them extensively.



This diagrammatic park layout shows how neighborhood parks can provide a variety of recreational and leisure areas, as well as connecting walking paths.



Walkability
Pedestrian-Friendly Environment
Connectivity

Identity
Public Art
Sustainability

Guidelines

- » Locate parks within 1,000 feet of new residential development to provide easy access to local residents.
- » Place parks in a highly visible location to encourage usage by non-local residents and to promote activity, thus increasing safety.
- » Parks should face onto public streets or pathways on at least two sides, and preferably on all four sides, to clearly define them as public space.
- » Parking should be provided in parallel parking spaces along the park perimeter. Leave gaps to provide views in to parks by police.
- » The park program should provide a variety of uses including active and informal areas that allow for various experiences and activities for people of all abilities.
- » Public art should be integrated into the design to reinforce a sense of the neighborhood.
- » Provide wind protection and shading in some areas, particularly at picnic facilities, through appropriate building placement, trees, landscaping, or shade structures.
- » Neighborhood parks should be mainly landscaped; hardscaped areas should be kept to a minimum.
- » Typical Neighborhood Park elements include:
 - » Children's playground
 - » Basketball court
 - » Picnic facilities
 - » Open turf area
 - » Tennis court
 - » Bocce/horseshoe play area
 - » One sports field (e.g. soccer or softball) in larger neighborhood parks



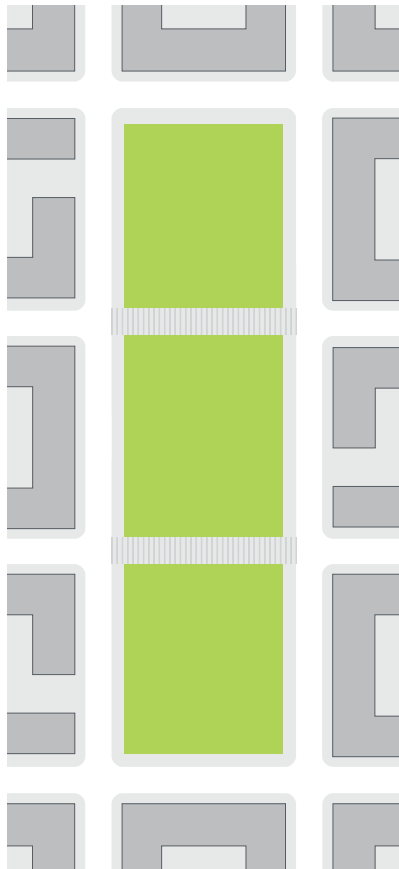
Promote family use with safe and visible children's playgrounds that are integrated into the park design.



Encourage activities for all ages by providing well-designed sports fields and courts.

Community Park

Provide a large community park, located close to residential neighborhoods and transit, that offers a variety of recreational and leisure opportunities.



Increase street frontages and visibility by placing the park on a narrower piece of land.

Discussion

A large community park or two smaller parks of equal acreage will give North San José a place for more intense sporting and recreational activities, providing a counterpoint to the civic focus of the urban plaza (discussed in the *Central Urban Park/Plaza* guideline in this section). The park should provide multiple sports fields and substantial community facilities, as well as areas of non-programmed green space for more informal recreational uses. Such a park can serve as a green oasis amid the more urban development in North San José. Since it is meant primarily to serve North San José residents, the community park should be placed closest to residential neighborhoods. Strategically locating the park along the San José trail network and within walking distance of transit will increase access to the park for all city residents.

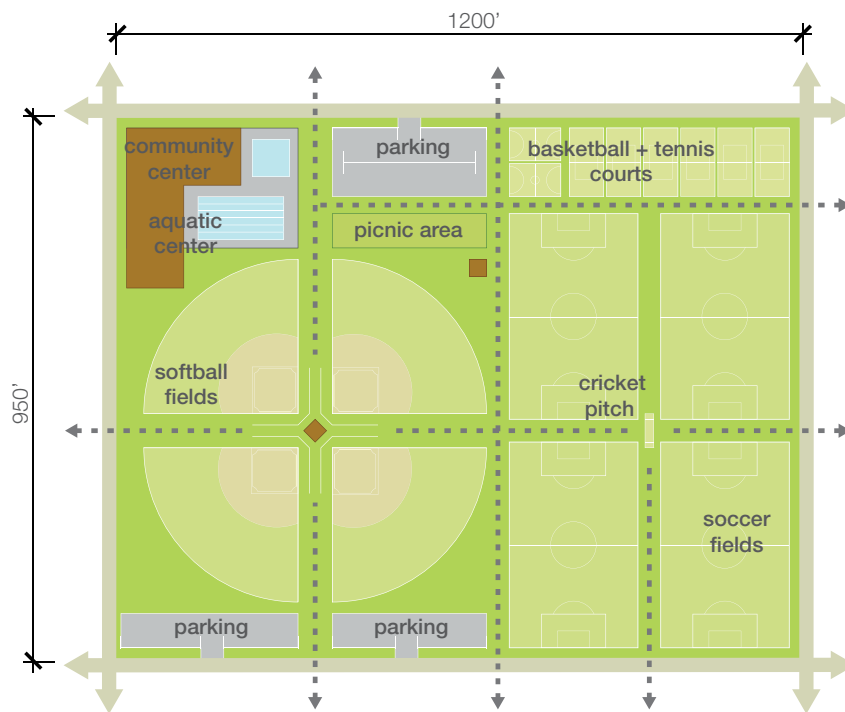
Guidelines

- » Integrate the park into the urban fabric and locate it within walking distance of some of the larger residential neighborhoods and public transit routes.
- » Locate the park close to one of the pedestrian/bicycle routes which cross North San José.
- » If a suitable site can be identified, the community park should be a single large park in the range of 35 and 40 acres.
- » If it is more practical to subdivide the park, it should be divided into no more than two separate parks as follows:
 - » The parks should each be approximately 20 acres in size.
 - » They should be placed to provide fairly equal access to residents in the North San José area; activities and uses should be fairly distributed between the two parks.
 - » The parks may be connected by a parkway to support their use as a single event venue.



Pedestrian-Friendly Environment
Connectivity
Identity

Public Art
Sustainability



This diagrammatic layout for a community sport park only includes a community center, aquatic center, picnic area, basketball and tennis courts, soccer fields, a cricket pitch, and softball fields within a 26-acre site.

Guidelines (continued)

- » The park should face onto a public street or pathway on at least three and preferably on all four sides.
- » Provide secure bicycle parking adjacent to park facilities and throughout the park.
- » Street parking should be provided around the park perimeter, and on-site parking should be located near activity nodes.
- » Locate larger facilities such as a community center or aquatic complex along the edges of the park, and closest to transit connections.
- » Orient park facility entrances to a public street or pathway, and integrate facilities' outdoor areas into the park setting.
- » Along with restrooms, provide electrical and water hookups to support a snack bar or café, to permit and encourage longer visits to the park.

Guidelines (continued)

- » Make the park accessible from all sides and place main entrances along public streets.
- » Provide electrical hookups and other infrastructure (for example, wireless internet access) for stage areas to encourage outdoor events.
- » Design a pedestrian pathway system that allows for direct connections through the park to all activity areas, and also to the public trail network.
- » Include active and informal recreational areas throughout the park.
- » Provide trees and shade structures, particularly in picnic areas and by play areas.
- » Incorporate public art that reinforces a sense of place and enhances engagement.



Locate large park facilities like a community center along the edge of the park to maximize accessibility.



The community park can contain larger facilities such as an aquatic complex.



Pedestrian-Friendly Environment
Connectivity
Identity

Public Art
Sustainability

Guidelines (continued)

- » Typical Community Park elements include:
 - » Community center
 - » Community gardens
 - » Group picnic areas
 - » Café or snack bar
 - » Enclosed, off-leash dog parks
 - » Indoor and outdoor aquatic facilities
 - » Gymnasium with two indoor basketball courts
 - » Sports fields – baseball, soccer, cricket, volleyball, etc.
 - » Basketball and tennis courts
 - » Skate park
 - » Walking/jogging paths
 - » Restrooms and changing facilities



Include a variety of recreational opportunities, like the skate park shown above, to serve a diverse population with a wide range of abilities.

Central Urban Park/Plaza

Provide a central park or plaza in the Core Area that defines an identity and sense of place for North San José.



Provide built elements that encourage activity, such as water features and seating.



Include unique design elements and utilize a mix of materials.

Discussion

A well-proportioned park or plaza, centrally located and clearly defined by surrounding buildings of an appropriate scale, can create an “urban living room” where North San José residents, employees, and visitors can gather and meet. The design and location of such a place should take into consideration intended uses, proximity to public transportation routes, relationships to other public open spaces, and the current configuration of land ownership. Its open space, facilities, and landscaping should be able to accommodate large-scale uses such as performances or temporary outdoor markets, as well as smaller-scale activities that will occur on a more frequent basis, in order to serve as a gathering place for all.

Guidelines

- » The urban park/plaza should be roughly three acres in area and should be placed in a central location in North San José’s Core Area.
- » Locate the park/plaza along North First Street, and also along at least one but preferably two other public streets.
- » Provide connections from the park/plaza to nearby plazas, paseos, pathways, and trails.
- » Encourage high-density mixed-use development along the perimeter of the park/plaza to create an enclosed urban space.
- » Keep building heights lowest along the southern edge of the park/plaza to allow for direct sunlight during most daytime hours.
- » Encourage pedestrian-friendly, interactive uses such as retail, restaurants, and cafés for the ground-floor uses of surrounding buildings.
- » Encourage such uses to ‘spill out’ into and engage the park/plaza, for example through café seating or outdoor merchandise displays.
- » Provide spaces that support flexible rather than fixed program elements where possible within the park/plaza.



Pedestrian-Friendly Environment
Mix of Uses

Identity
Public Art



The plaza should accommodate large-scale events such as performances and rallies.



Include a large-scale public art piece with iconic qualities that reflects North San José's innovative spirit.

Guidelines (continued)

- » Provide larger-scaled hardscaped and softscaped areas to accommodate events like concerts, performances, parades, farmers' markets, rallies, and film screenings.
- » Provide a variety of smaller-scaled seating areas and shade structures for day-to-day use.
- » Design for both daytime and evening use.
- » Incorporate a large-scale public art piece that has iconic qualities and reflects North San José's spirit of innovation. Also create opportunities for temporary art.
- » Typical urban park/plaza elements include:
 - » Amphitheater seating with shade
 - » Interactive water feature
 - » Major public art element
 - » Special plaza lighting
 - » Display area and stage
 - » Concession stands and restrooms

Trails

Continue to develop and strengthen the trail network in North San José.

Discussion

Trails are an enjoyable recreational and commuting resource, offering residents healthy opportunities for walking, running, bicycling, and skating, and the chance to connect to nature while in the city. The existing trail network in North San José is already quite popular with San José residents and workers. Along with trails, additional connections are provided by parkways that are a part of the street network (see *Guidelines for the Streetscape: Street Hierarchy and Typologies: Parkways*).

The existing network includes the Lower Guadalupe River Trail, Highway 237 Bikeway, Coyote Creek Trail, and partial development of the Hetch-Hetchy Trail. These trails define paths along the edges of, and through, North San José. By upgrading and completing connections along the existing trails, and by developing new opportunities, a comprehensive and interconnected network can be developed. A new trail opportunity, Component Trail, has been identified. The trail is shown, along with the existing and planned network, in the diagram below. Such a network will offer a unique way for people to experience North San José.





Walkability
Pedestrian-Friendly Environment
Connectivity

Identity
Public Art
Sustainability

Guidelines

- » Design trails to accommodate both pedestrians and bicyclists.
- » Complete and strengthen access points to the Lower Guadalupe River and Coyote Creek trails via stair/ramp connections, linking to parks, major development sites, and parkways.
- » Connect parks, plazas, open spaces, and transit stations to the trail network; use public art to reinforce these connections.
- » Identify opportunities to connect the trail network to existing and planned trails beyond the North San José and City of San José boundaries, for example to the San Francisco Bay Trail.
- » Developments may be required to provide trail connections if a trail runs near or adjacent to their parcel.
- » Complete planning for and construct a paved trail along the full length of Coyote Creek, linking it to the Highway 237 Bikeway and San Francisco Bay Trail to the north, and to planned reaches to the south.
- » Complete paved trail improvements along the Lower Guadalupe River Trail, and link it to the San Francisco Bay Trail.
- » Complete improvements to the Highway 237 Bikeway, including realignment to remove the detour at North First Street; and a north-side reach to connect the system to the Coyote Creek Trail and San Francisco Bay Trail.
- » Extend the Hetch-Hetchy Trail along the San Francisco Water District right-of-way by improving and expanding its presence along and through existing private parking lots. Construct stair/ramp access points to connect the trail to the Lower Guadalupe River and Coyote Creek Trails.
- » Align the Component Trail with Component Drive to provide a linkage to the North First Street light rail station. Construct a stair/ramp access point at the western end of the trail to connect it to the Lower Guadalupe River Trail.
- » Construct stair/ramp access points on the Coyote Creek Trail to connect it to the planned Charcot Parkway and to the River Oaks Parkway.



Trails can connect to and highlight local waterfronts and other natural areas.



Trails offer unique recreational and commuting opportunities for North San José residents.

Trail Design

Develop and implement trail designs that take into consideration primary users, the natural environment, and the built context.

Discussion

Trail network users include commuters, exercisers, and those simply seeking out nature in the city. Some trails may pass by sites of ecological, historical, or cultural importance. And trails pass through a variety of settings, from more natural areas along the rivers, to leafy residential neighborhoods, to the most urban areas of North San José, along North First Street in the Core Area. Regardless of destination or adjacency, adherence to common design guidelines ensures that users will find trails of similar quality and functionality.



Design trails to the City's standards for minimum width and materials to ensure consistent quality and functionality throughout the trail network.



Walkability
Connectivity

Identity
Sustainability

Guidelines

- » Trails should be designed to the City's standard for minimum trail width.
- » Trails should be located at a sufficient distance from residential and commercial developments to help ensure property owners' privacy and to protect the environmental quality of the trail.
- » Take advantage of landscaping opportunities for trail segments that run along the edges of parks or non-sensitive open spaces.
- » Where trails cross the existing road network, consider grade-separated crossings for increased safety.
- » Use high-quality materials consistently, and reflect the surrounding design aesthetic when selecting materials.
- » Include public art throughout the trail network, and integrate it with other design features such as seating areas, gateways, access points, and signage; also use it to create points of interest and engagement.
- » Gateway structures or other vertical landmark elements such as view platforms should be used to mark important locations, particularly at waterway access points.
- » Signage along trails should address the following issues: identification; wayfinding; safety/regulations; mileage/location; and directional and interpretative information. Build on existing signage design for future trail signage.
- » Include milestone-marker signage to support recreational goals, rapid deployment of emergency services, and incident recording.

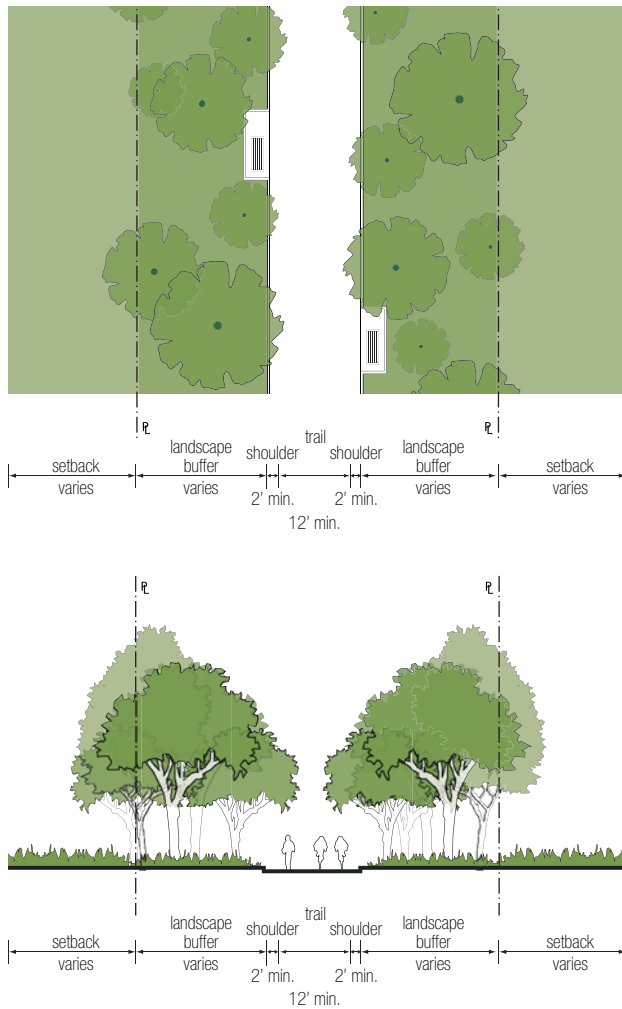


Easy-to-read signage helps orientation and contributes to identity.



Educational displays can become points of interest for trail users.

Trail Design



Typical Trail

Guidelines (continued)

Typical Trail

- » The typical trail is a paved trail, 12 feet wide with a 2-foot shoulder on either side, shared by pedestrians and bicyclists, that allows for emergency vehicle access.
- » Trails should comply with the County of Santa Clara Trail Design Guidelines (or the City of San José's Guidelines once developed), and Chapter 1000 regarding bikeways in the California Department of Transportation (Caltrans) Highway Design Manual.
- » Adjacent areas sometimes contain lush vegetation that serves as an additional green buffer. Such vegetation should be allowed and encouraged where feasible.



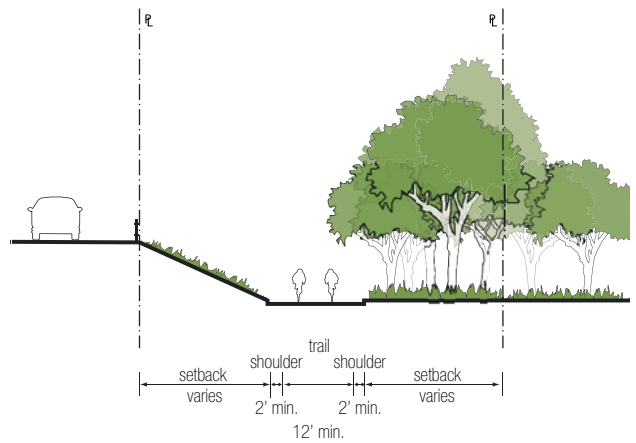
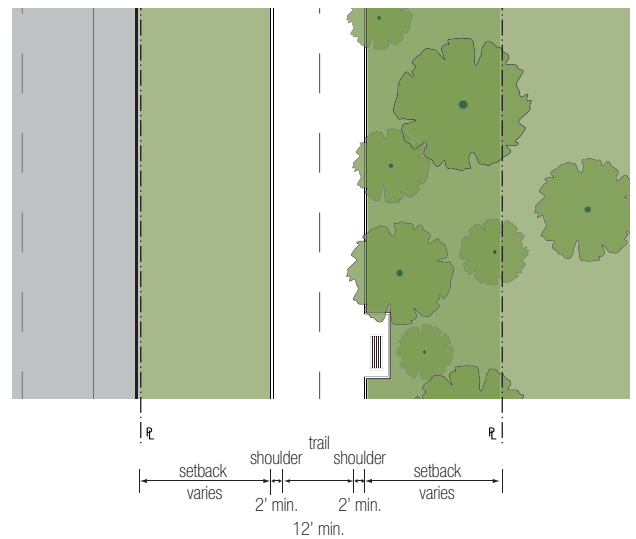
Walkability
Connectivity

Identity
Sustainability

Guidelines (continued)

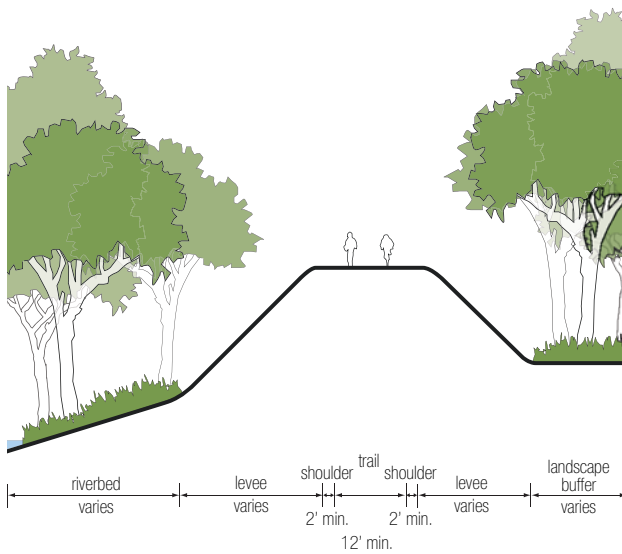
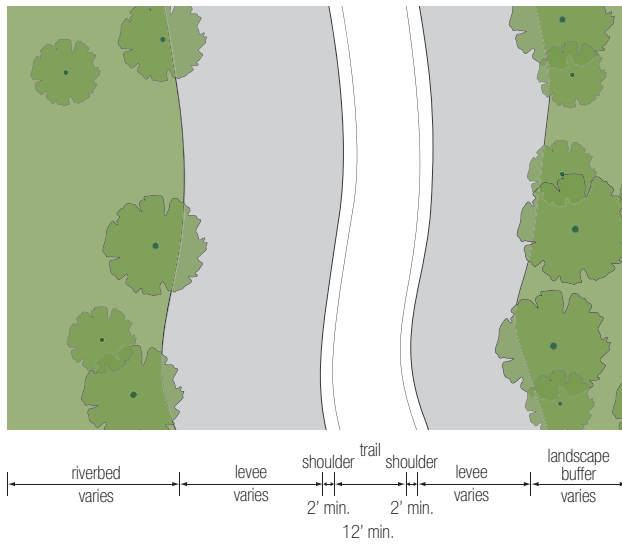
Highway 237 Bikeway

- » This bikeway is a paved trail for use by bicyclists and others.
- » Currently much of the bikeway is edged with chain-link fencing that should be removed, if possible. If a fence is necessary for safety purposes, replace the chain-link fence with a more aesthetic solution.
- » As new development occurs along the bikeway, efforts should be made to make it more open and park-like, as shown.



Highway 237 Bikeway

Trail Design



Lower Guadalupe River and Coyote Creek Trails

Guidelines (continued)

Lower Guadalupe River and Coyote Creek Trails:

- » The existing gravel trails will be developed as a paved trail, 12 feet wide with a 2-foot shoulder on either side, shared by pedestrians, bicyclists, and others. This design allows for emergency services and maintenance vehicles.
- » For the most part, these trails are located on top of engineered levees that provide a flood-protection function.
- » Due to the engineered nature of the levee, landscaping or vegetation is not allowed directly on it except for grasses, which are permitted on the levee sides.
- » Levee-adjacent areas sometimes contain lush vegetation that serves as an additional green buffer between the waterway and adjacent development. Such vegetation should be allowed and encouraged where feasible.



Walkability
Connectivity

Identity
Sustainability



Guidelines for Public Art

Public Art

Public Art

Incorporate public art into both private and public development to contribute to North San José's distinct character and identity.

Discussion

The City of San José values public art as a reflection of its creative character. Public art in North San José can enrich the public realm, capturing the changing character of the area and contributing to its visual legibility. Public art can also reinforce the vision of North San José as a center for innovation and a community that is becoming more urban and sustainable. Artworks can be commissioned to reinforce the goals of these guidelines and to create landmarks, opportunities for community interaction, and human-scaled places.

The placement of public artworks in North San José will be determined through an area-wide strategy that identifies the best opportunities. Public art projects funded through eligible City of San José and eligible Redevelopment Agency capital construction projects will be commissioned for trails, parks, streetscape elements, and schools. Public funds will also be pooled to commission prominent public artworks of area-wide significance. Private developers will be encouraged to voluntarily integrate permanent and temporary public art into communal spaces at their retail, commercial, and residential development projects, or to contribute to public art pooled funds for the creation of significant public artworks.



Public art of various scales can stimulate community interaction.



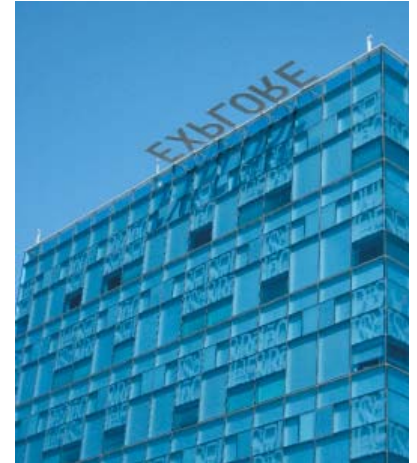
Sculptures can be placed at highly visible locations and can become a part of the streetscape. They can be standalone pieces or part of a series distributed over a larger area.



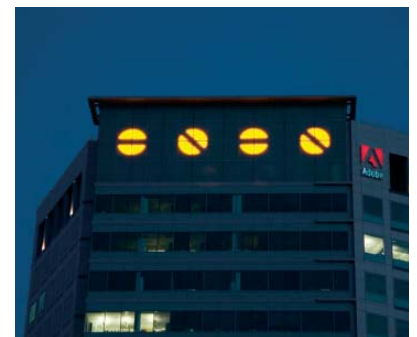
Pedestrian-Friendly Environment
Identity
Public Art

Guidelines

- » Commission public art that captures the character of North San José, reinforcing its identity as the innovation capital of Silicon Valley.
- » Locate public art to mark key paths of movement (such as trails, corridors, and connections), to highlight major entries (to both North San José as a whole and to specific sub-areas), and to anchor key spaces.
- » Commission public artworks that act as “community hearths”, stimulating interaction where people of different communities or user groups meet.
- » Commission public artworks at a variety of scales:
 - » large-scale “City Image” projects that create the “postcard” image that people think of when they think of North San José;
 - » area-scale projects that provide orientation and identity to different sub-areas in North San José; and
 - » neighborhood-scale projects that relate to the way that people work and live in North San José.
- » Create “strong spots” and “hot spots” for the placement of temporary public artworks, focused on gathering spaces and pedestrian-oriented experiences, that create a sense of excitement and expectation.
- » Locate public art in interstitial places, weaving together zones where different kinds of uses overlap, such as places where parks and schools, businesses and residential areas, or transit and pedestrian areas meet.
- » Use public art to enhance the trail system, creating unique artworks at areas where trails meet parks or schools; also include smaller-scaled functional and interpretive art elements along the trails.



Building facades can integrate subtle public art pieces.



Playful and changing public art such as LED lighting and can make use of different conditions during day and night.

Glossary, Acknowledgements, and Credits

4

Glossary

Build-to Line: A line with which the exterior wall of a building must coincide. Minor variations from the build-to line are allowed for features such as recesses, projections, or other façade articulations.

Building Articulation: The visible expression of architectural elements through form, structure, or materials that break up the scale of buildings and spaces.

Community Park: Community parks in San José are typically seven acres or larger and may include features such as multiple sports fields for free and league play, community centers, aquatics facilities, picnicking areas, play areas, sports courts, and community gardens.

Encroachment: A variation in a building's façade where a portion of the building hangs over the defined setback or build-to line.

Mid-Block Connection: A narrow pathway, typically around 20 feet to 40 feet wide, that offers pedestrian and bicycle access through the center of a block.

Neighborhood Park: Neighborhood parks in San José are typically one to seven acres in size and include features such as play areas, picnic areas, basketball courts, open turf, and if large enough, sports fields.

Parkway: A public street with a wider cross-section to permit development of a landscaped pathway that supports bicycle and pedestrian activity.

Paseo: An publicly-accessible street, a minimum of 60 feet wide and designed to City street standards, that is pedestrian-only except for emergency vehicular access.

Pedestrian-Scale: The size and proportion of a physical element that closely relates to the human body; for example a 16-foot lamppost versus a 30-foot lamppost.

Permeable or Pervious Material: Materials that allow water to enter the ground due to their porous nature.

Pocket Park: A privately-owned and -managed but publicly-accessible urban park, typically no larger than one acre in size. Pocket parks are the smallest type of open space, and can accommodate seating areas for relaxing or eating lunch, small performance areas, information kiosks, fountains, and public art.

Podium Parking: A structured parking facility that contains parking either at grade or partially below grade, on top of which a building is built.

Recess: A variation in a building's façade where a portion of the building is stepped back from the defined build-to line.

Setback: The minimum distance between a building façade and the property line of the parcel on which it is located.

Trail: A paved or gravel surface that meets California Department of Transportation Class I bikeway standards and supports commuting and recreational uses by pedestrians and bicyclists. Trails are a minimum of 12 feet wide, with a 2-foot wide shoulder on either side. Trails are independent of the public street system.

Acknowledgements

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Chapter 2: Urban Design Framework Plan

- p. 11: *Circuit Board* by Peter Shanks
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Residential Neighborhood Street by Kyle Gradinger

Chapter 3: Urban Design Guidelines:

Site Planning

- p. 30: *40-foot Mid-block Connection* by Kyle Gradinger
- p. 31: *20-foot Mid-block Connection (Residential)* by Kyle Gradinger
- p. 50: *Mixed-Use Street* by Joel Mann

Buildings

- p. 69: *Office Building Atrium* by Carlos Morel
Exterior Shading Devices by William Murphy
- p. 70: *BMW Headquarters Façade* by Evan Chakroff

Parking

- p. 79: *4th Street Garage Façade* by Patrick Boury

Streetscape

- p. 109: *Planted Bulb-Out* by Richard Drdul

Public Art

- p. 137: *The Docklands Plaza at Night* by William Murphy

