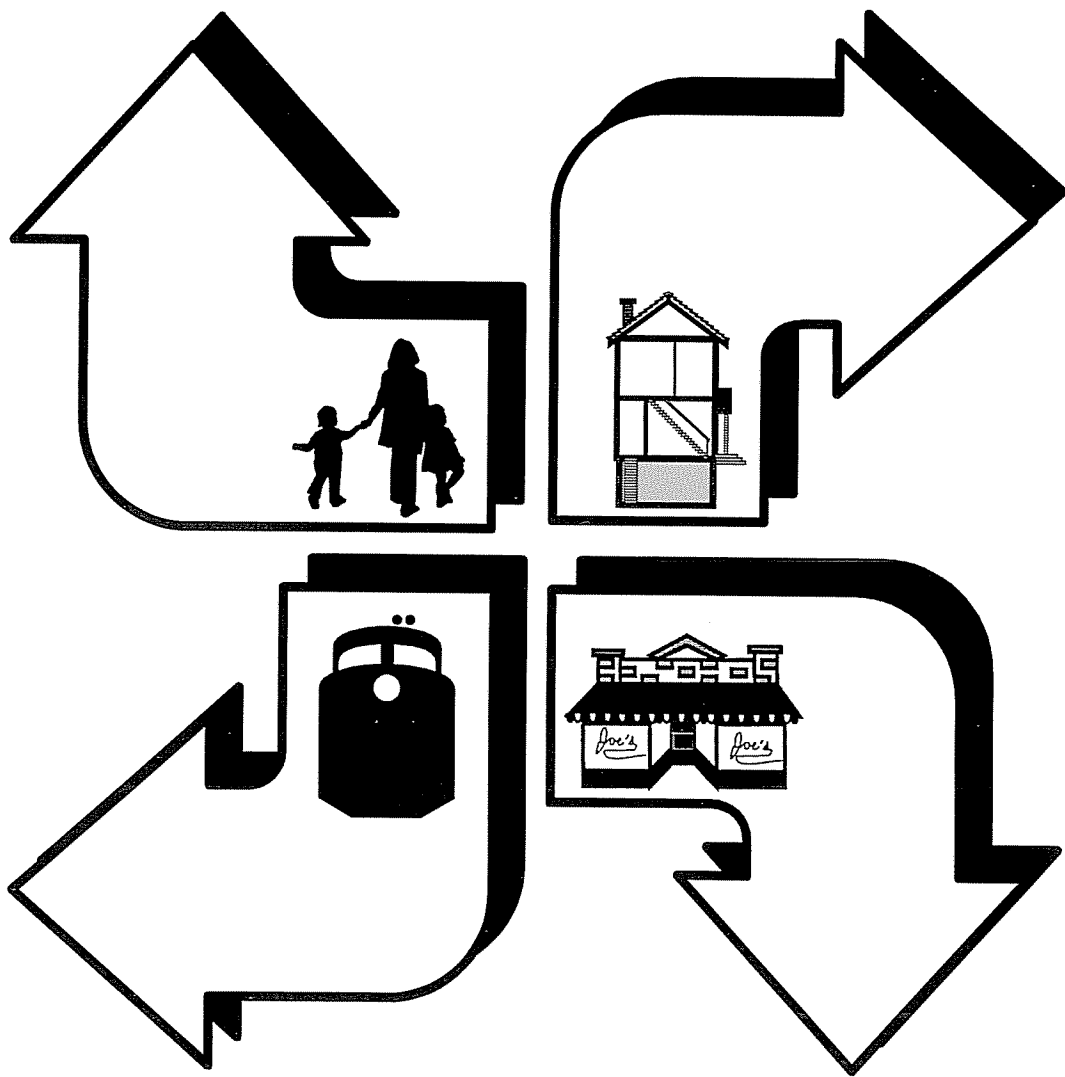


TAMIEN STATION AREA SPECIFIC PLAN



City of San Jose
Adopted March 21, 1995

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City of San Jose

Department of City Planning and Building

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INTRODUCTION

Summary

The Tamien Station area presents a unique opportunity to take advantage of not one but two major transit facilities: the Tamien Light Rail Transit Station (a portion of the Guadalupe Corridor) and the Tamien CalTrain Station. These two facilities together constitute the Tamien Multi-Modal Station which was completed in the late 1980s and is an important transit hub combining bus service as well as heavy and light rail service. The Tamien Station area also has convenient access to State Route 87 for automobile users. This combination of transportation facilities provide a direct link to employment centers in the Downtown, which is two thirds of a mile to the north, and in North San Jose. Vacant and underutilized lands in the Tamien Station area could be developed or redeveloped with high density housing and mixed uses to create a viable and unique transit and pedestrian oriented neighborhood.

San Jose's Housing Initiative Program recognized the exceptional development opportunities possible in the Tamien Station area during the preparation of the Housing Initiative Study for the Guadalupe Corridor (the first phase of the program) which was begun in 1989 and completed in early 1991.

The purposes of the program include:

- 1) encouraging the production of high density housing for all income levels;
- 2) encouraging public transit use;
- 3) locating housing near job centers;
- 4) optimizing the service capacity of existing infrastructure;
- and, 5) encouraging efficient use and reuse of land. To take advantage of these

opportunities and achieve the purposes of the Housing Initiative Program, the City of San Jose chose to prepare a specific plan to create a transit oriented community tailored to the Tamien Station area.

The Tamien Station Area Specific Plan translates the objectives of the Housing Initiative into specific land uses and policies consistent with the opportunities and constraints of the Tamien Station area. The goals and objectives will be achieved by implementation of the policies specifically designed to support them. In particular, the specific plan will guide the future use of vacant or underutilized non-residential lands to create a vital, high density residential and mixed use community that is transit oriented and is compatible with the surrounding neighborhood. The specific plan also strives to create a livable community that is pedestrian oriented and supported by an improved pedestrian circulation system. A mix of uses is encouraged that will allow for the convenient location of commercial and service uses so that local residents can satisfy day-to-day shopping needs by walking rather than driving. The specific plan contains land use and design policies intended to create a unique urban setting that will also strengthen the positive characteristics of adjacent neighborhoods.

The intent of the specific plan is to encourage investment and new development within the Tamien Station area through a clear public commitment to a new transit and pedestrian oriented community, and through a definitive pattern of land uses that provide property owners with a level of certainty regarding the future form and character of new development. It is not the intent of the plan to force existing

businesses out of the area; rather, the plan provides policies to reduce potential conflicts between new development and existing uses. The plan acknowledges that the full buildout of the Tamien Station Area Specific Plan will take many years to occur, and that to avoid piecemeal decisions and lost opportunities, the specific plan must clearly describe the desired vision and character of the future community.

More specifically, the Tamien Station Area Specific Plan provides for:

- The intensification of development immediately adjacent to the Tamien Multi-Modal Station with a mixture of high density residential and commercial uses that will create a resident population in close proximity to transit, and that will strengthen these areas as pedestrian oriented activity centers.
- The creation of a new residential community of up to 1,225 new dwelling units, in addition to the existing 457 dwelling units located in the plan area, offering a wide range of housing choices including apartments, condominiums, and townhouses. A major objective of the plan is to meet a variety of housing needs.
- The creation of an improved system of pedestrian ways and open spaces that promotes the Tamien Station area as a livable and walkable community.
- The compatible relationship of the Tamien Station area with

surrounding areas and neighborhoods. Through comprehensive urban design policies, the plan establishes an appropriate scale and pattern of development adjacent to existing neighborhoods.

Scope of the Specific Plan

The Tamien Station Area Specific Plan establishes the location, intensity and character of land uses, maintains the existing vehicular circulation pattern, proposes improvements to enhance pedestrian activity, establishes the location and configuration of parks and community facilities within the area, and describes the implementation actions required to realize the plan's objectives. As such, the specific plan provides detailed policy direction, elaborating upon the San Jose 2020 General Plan which is the principle document governing long term growth and development within the City.

The Tamien Station Area Specific Plan is incorporated into the San Jose 2020 General Plan as the Tamien Station Area Planned Community. The General Plan describes the land uses permitted within the Tamien Station area and incorporates the major objectives and policies described in this plan.

A Final Environmental Impact Report (EIR) was certified for the Tamien Station Area Specific Plan in conformance with the California Environmental Quality Act (CEQA).

Organization of the Specific Plan

The Tamien Station Area Specific Plan is composed of eight chapters. Chapter 1, INTRODUCTION, summarizes the content and scope of the specific plan, explains its general organization, and summarizes the planning process, including Task Force participation, and initial objectives that were used to guide the preparation of the plan.

Chapter 2, BACKGROUND, provides an overview of the Tamien Station area. It also describes the opportunities and constraints of the Tamien Station area which form the basis for the development of the plan.

Chapter 3, GOALS AND OBJECTIVES, discusses the major strategies or themes of the Tamien Station Area Specific Plan which are expressed as goals and objectives and include supporting transit use and creating a pedestrian friendly environment, creating a livable community, providing housing opportunities, and ensuring neighborhood compatibility.

Chapter 4, LAND USE PLAN AND LAND USE DESIGNATIONS, describes the land use plan for the specific plan, explains the plan's land use designations, and describes the three subareas which compose the Tamien Station area: Transit Core, Guadalupe River, and Almaden Road.

Chapter 5, LAND USE AND DESIGN POLICIES, sets forth the land use and design policies for the specific plan's three subareas.

Chapter 6, STREET AND CIRCULATION SYSTEM, contains policies designed to improve pedestrian and vehicular circulation.

Chapter 7, COMMUNITY FACILITIES, SERVICES, AND UTILITIES, describes the various community facilities and urban services planned to serve existing and new development.

Chapter 8, IMPLEMENTATION, explains the processes required to carry out the Tamien Station Area Specific Plan and contains additional policies for achieving the objectives of the plan. It also describes how the specific plan is consistent with the San Jose 2020 General Plan and other City policies and how it should be administered in the future.

The Planning Process

The planning process for the Tamien Station Area Specific Plan was initiated in early 1993 when the City Council approved a budget for preparation of the specific plan. In September 1993, the City Council selected a nineteen-member Task Force composed of property owners, business persons, local residents, and agency representatives to assist in the preparation of the specific plan. From October 1993 to January 1995, the Task Force held public meetings to consider the direction of the specific plan.

The Task Force began by reviewing and discussing background information concerning land use, transportation facilities, community services and facilities, demographics, infrastructure, and environmental issues. They were also briefed by various public agencies and City departments on housing, park facilities, crime statistics, flood control, and transportation facilities. The Task Force discussed desired community character and

plan objectives while considering the opportunities and constraints in the Tamien Station area. After evaluating three land use alternatives, the Task Force selected a preferred land use alternative which reflected its vision for a planned community in the Tamien Station area. The Task Force then discussed, evaluated, and selected a series of policies to achieve the objectives of this plan. The resulting document was then presented to the Planning Commission. At the suggestion of the Planning Commission, the Task Force considered a reduced density alternative which became the basis for the Plan. This revised Plan was submitted to the City Council for consideration and action.

Task Force Objectives

The Task Force articulated its vision for the Tamien Station area in a series of general objectives that were used to guide the development of the Tamien Station Area Specific Plan. These objectives encompass concepts such as neighborhood preservation, housing opportunities, environmental protection, recreational facilities, and urban design as summarized below.

Neighborhood Preservation

1. Maintain the positive qualities of existing neighborhoods by ensuring that new development is compatible with surrounding neighborhoods.
2. Unite the diverse areas composing the Tamien Station area to create a unique community.
3. Preserve neighborhood safety by mitigating the potential traffic

impacts generated by new development.

4. Design new development to encourage residents to interact, and to foster pedestrian and neighborhood activity.

Housing Opportunities

5. Encourage a wide variety of housing opportunities for all income levels on vacant and underutilized lands in the Tamien Station area.
6. Explore innovative methods to finance new housing construction.

Environmental Protection

7. Protect the Tamien Station area from potential environmental hazards associated with noise, vibration, flooding, traffic, and hazardous materials.
8. Identify potential sources of groundwater contamination and apply appropriate mitigation measures to new development that would substantially eliminate this contamination.
9. Measure noise levels generated by transportation activities and develop appropriate mitigation measures to create a comfortable noise environment.

10. Determine potential flood hazards and mitigate flood hazard impacts that may affect new development.

Recreational Facilities

11. Provide park, recreation, and trail facilities to serve the residents of the Tamien Station area.
12. Identify park needs and determine how to best meet these needs.
13. Explore the feasibility of providing a new park adjacent to the Guadalupe River.
14. Consider constructing a river walk along the Guadalupe River to provide an attractive pedestrian amenity.

Urban Design

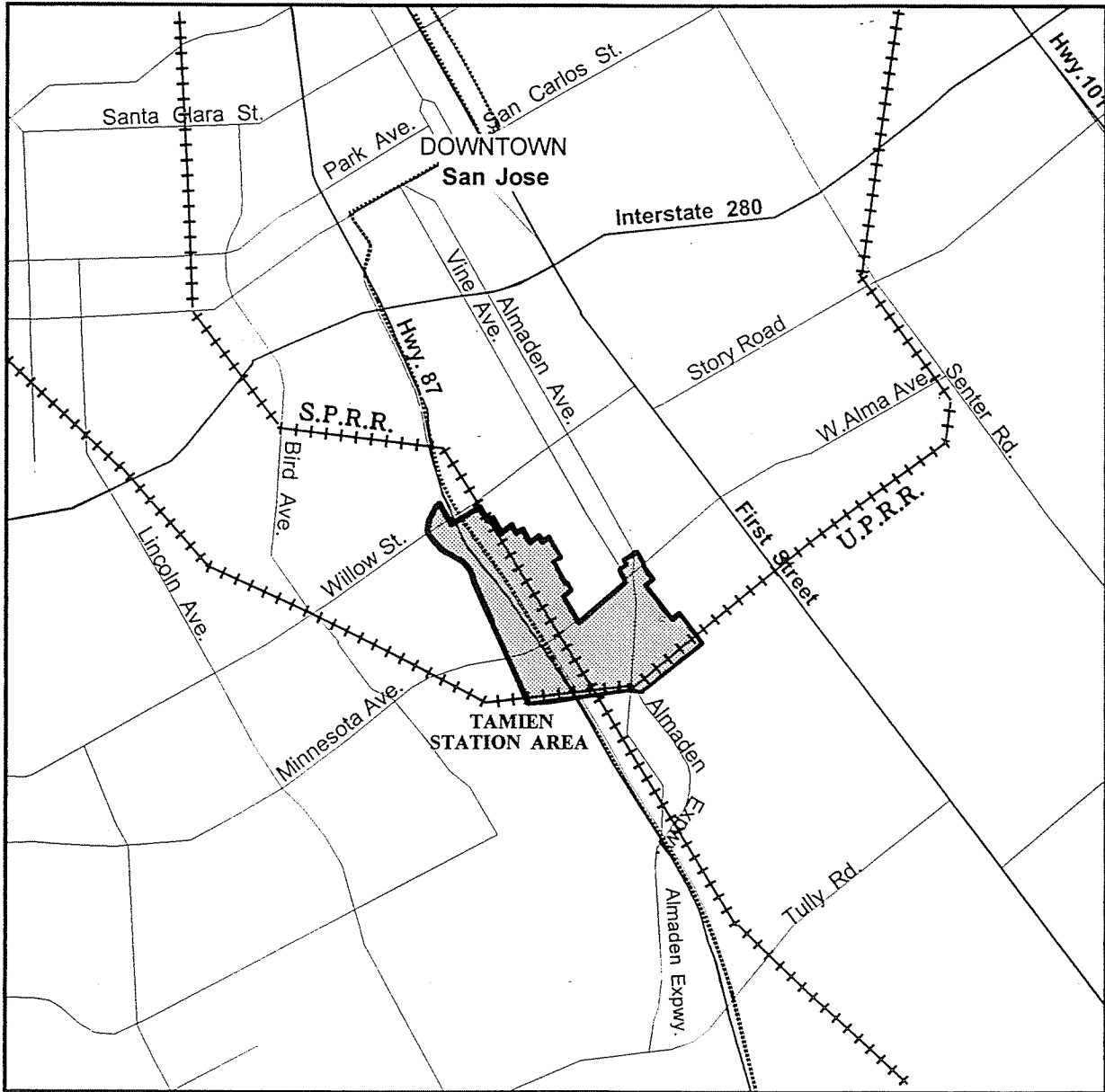
15. Create safe and convenient pedestrian access which links new and existing residential and commercial development to Tamien Station.
16. Improve the appearance of existing streetscapes to generally upgrade the area.
17. Incorporate a high level of architectural and urban design quality in all improvements and development projects proposed in the Tamien Station area.
18. Create an unique identity for the Tamien Station area through a unified landscape plan, public art,

street furniture and other improvements.

19. Explore the use of air rights development over Tamien Station to provide additional improvement opportunities in the area.
20. Locate shops, restaurants, and entertainment uses in close proximity to Tamien Station to enable transit users and residents to use services without driving.

VICINITY MAP

Figure 1



SCALE: 1"=2300'

BACKGROUND

Location

The Tamien Station Area Specific Plan covers an area of about 140 acres generally bounded by Willow Street to the north, Lick Avenue and Little Orchard Street to the east, the Union Pacific Railroad to the south, and the Guadalupe River to the west. The Tamien Station area is located about two thirds of a mile south of Downtown and is bracketed by the Gardner, Guadalupe-Washington, and Willow Glen neighborhoods. These are well established, older neighborhoods comprised primarily of single-family dwellings. An older industrial/commercial area is located just to the south of the specific plan boundary. The specific plan area contains portions of Council Districts 3, 6, and 7 which share common boundaries in the area.

Existing Land Uses

The most significant land uses in the area (see Figure 2) are those related to transportation facilities including the combined Route 87 freeway and the Guadalupe Corridor Light Rail Transit Line which bisect the Tamien Station area and are the dominant man-made features of the area. In addition to these facilities, the Tamien Station area contains the Southern Pacific Railroad and CalTrain Line, which run parallel to Route 87, and the Union Pacific Railroad which forms the southern boundary of the specific plan area. Besides these major transportation uses, a mix of commercial, industrial, public/quasi-public, and residential uses occupy the area.

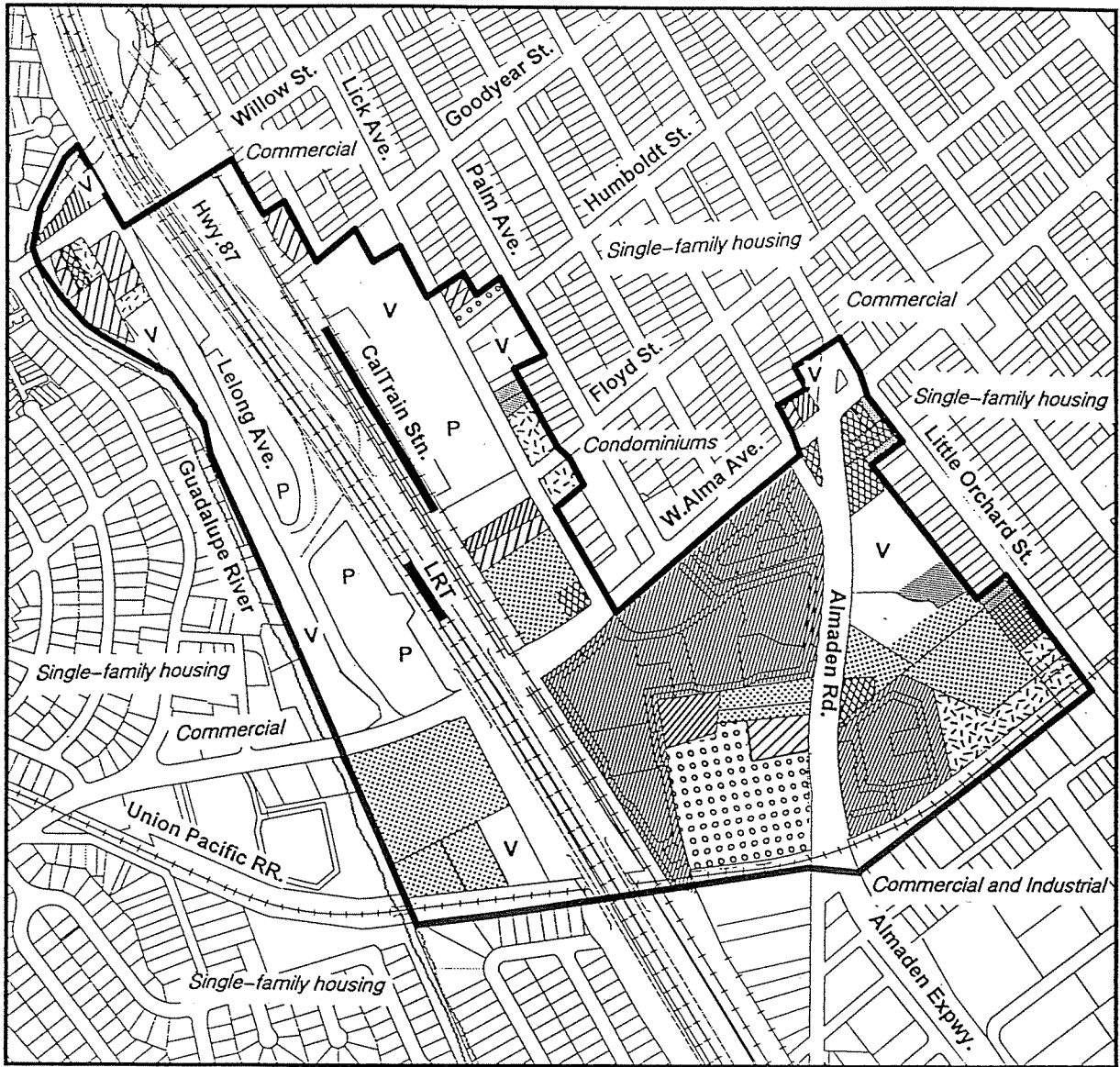
Commercial uses in the area generally consist of small retail and service uses including liquor stores, auto sales, bars, restaurants and offices. Two larger commercial uses, however, are locally significant. The Alma Bowl is a large recreational commercial facility located at the northwest corner of Lick and Alma Avenues and is something of a local landmark. Another landmark is the Italian Gardens banquet facility located on the east side of Almaden Road between Alma Avenue and the Union Pacific Railroad.












Industrial uses occupy about 21% of the Tamien Station area and consist primarily of manufacturing, warehousing/storage/distribution, and construction related uses. The largest industrial areas contain the combined Smith/Cristina properties located at the northwest corner of Almaden Road and the Union Pacific Railroad (UPRR) and the Stucco Supply site located at the northwest corner of Little Orchard Street and the UPRR. In addition to these relatively larger industrial sites, smaller industrial uses are located along Lick Avenue, Lelong Avenue, and Willow Street. The industrial uses along Lelong Avenue and Willow Street will eventually be eliminated by the construction of the Guadalupe River flood control bypass channel.

Public/quasi-public uses consist primarily of the transportation facilities described above, the Tamien Multi-Modal Station and the parking lots associated with it, and the Elks Club lodge facility. The parking lots are located on both sides of Route 87 with the eastern parking lot serving the CalTrain

EXISTING LAND USE

Figure 2



- | | |
|---|---|
|  Vacant |  Manufacturing |
|  Single-family Residential |  Warehousing/Storage/Distribution |
|  Multi-family Residential |  Auto/Truck/Equipment/Service or Sales |
|  Retail/Services/Office/Restaurant/Bar |  Construction-related industrial |
|  Recreation/Entertainment/Lodges |  Outdoor storage |
|  Parking lot | |



Scale: 1" = 750'

Station and the western parking lot serving the Tamien Light Rail Transit (LRT) Station. The Elks Club is located at the southwest corner of Alma Avenue and Route 87 just across the street from the LRT station and is another important local landmark.

The existing residential uses within the specific plan boundary consist of two large condominium projects and one apartment project. The two condominium projects are the Castle Garden (84 units) and Shadow Run (231 units) located on the south side of Alma Avenue between Route 87 and Almaden Road. The Cinnabar apartment complex contains 142 units and is located at the northeast corner of Almaden Road and the UPRR.

Environmental Setting

The name "Tamien" is the Ohlone regional name for the Guadalupe River area. The Guadalupe River (see Figure 3) is no less important for the Tamien Station area today than it was for the original inhabitants of the area. It is the most significant natural and open space feature of the Tamien Station area and is the only significant wildlife habitat in the area. It also represents a potential flooding hazard which the Santa Clara Valley Water District (SCVWD) is planning to control with a bypass channel for which construction is scheduled to begin in 1997.

Most of the Tamien Station area has been modified by the actions of man, first by agricultural uses, including orchards, and then by the canneries that followed the orchards. After World War II more manufacturing uses were located in the

Tamien Station area and other nearby areas such as the Downtown. Residential development also occurred during this period to house the workers generated by the cannery and manufacturing uses. This process of development virtually eliminated the natural features of the area.

The only significant trees in the area, outside of those located within the Guadalupe River riparian corridor, are located on the Italian Gardens site associated with a formal garden or the existing residence. The rest of the vegetation in the Tamien Station area is primarily limited to ornamental landscaping associated with residential development and some street trees of various types and conditions. There are long stretches of streets where street trees have been removed and not replaced both within and adjacent to the Tamien Station area. Little or no vegetation can be found on sites containing non-residential uses.

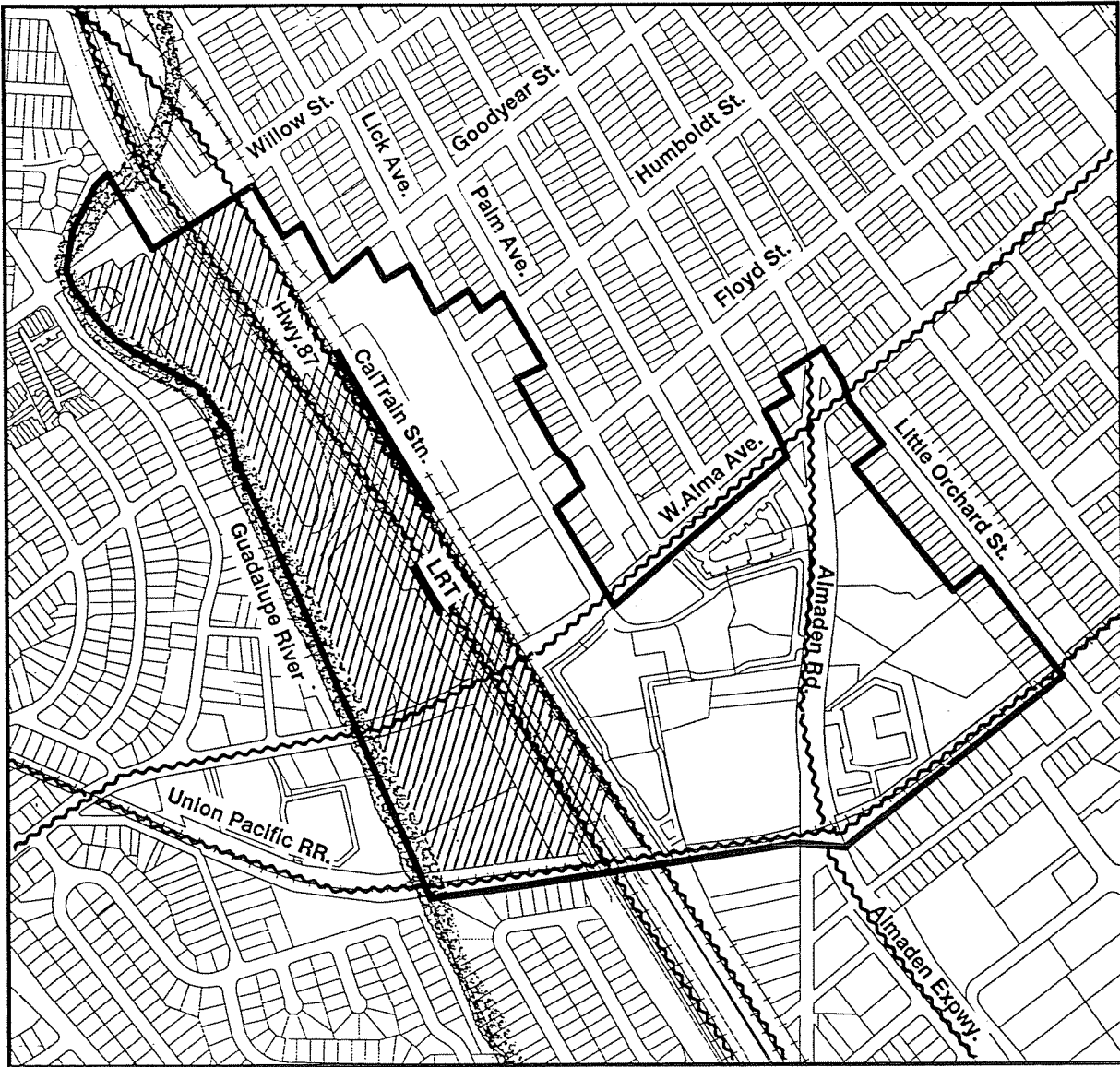
Urban development in the area has generated environmental impacts such as increased traffic and noise, reduced air quality, and potential exposure to hazardous materials from industrial activities. The major transportation facilities in the Tamien Station area generate most of the traffic, noise, and air quality impacts. Each of these impacts as well as the overall environmental setting were carefully considered in the preparation of the specific plan and in its Environmental Impact Report.




Opportunities and Constraints

The location, setting and conditions of the Tamien Station area presented a variety of opportunities and constraints for the

KEY ENVIRONMENTAL FACTORS

Figure 3



-  Noise/Vibration Source (Freeway, Light and Heavy Rail Lines, Major Roads)
-  Riparian Corridor
-  100 Year Flood Zone



Scale: 1"=750'

development of a specific plan all of which were evaluated by the staff and the Task Force. This section summarizes some of the key opportunities and constraints considered during the preparation of the Tamien Station Area Specific Plan.

Land Use Opportunities

1. The vacant and underutilized sites within the Tamien Station area represent a valuable resource for new development or redevelopment and reuse as high density residential or mixed use projects since most sites are located within walking distance of the Tamien Multi-Modal Station.
 2. The Tamien Multi-Modal Station parking lots need not be limited solely to parking lot use and can accommodate some level of intensification.
 3. New high density residential development can help support existing commercial uses and some new commercial uses, both within and outside of the specific plan area.
 4. The Tamien Station area is well suited to encourage a compact form of development that efficiently utilizes existing urban services and infrastructure.
2. Archaeological and cultural resources may limit the location and extent of new development to preserve or avoid damaging these resources.
 3. Land use conflicts may arise between existing industrial uses and new residential development.
 4. Portions of the Tamien Station area near the Guadalupe River may be subject to flooding.

Transportation Opportunities

Land Use Constraints

1. Existing hazardous materials contamination may limit the location and extent of new development or require substantial remediation on some sites.
1. The Tamien Multi-Modal Station provides access to the Guadalupe Light Rail Transit (LRT) Corridor connecting north and south San Jose, to the CalTrain line providing service from Gilroy to San Francisco, to the Amtrak Line (when the LRT line to Cahill Station is completed), and to Santa Clara County Transit District bus service. This combination of transit facilities provides ready access to employment centers and can help reduce dependence on the automobile. This could in turn make the Tamien Station area an attractive location for housing by those employed in the Downtown or employment centers further north.
 2. The Tamien Station area has excellent access to the Route 87 freeway and from Route 87 there is convenient access to Highways 280, 680, and 101. This situation provides an opportunity for those who choose not to use transit to use the most efficient automobile routes.

3. The existing grid street pattern and system of sidewalks provides flexible and direct pedestrian and bicycle access to the Tamien Multi-Modal Station. The existing bicycle access to the station will be improved when the City's draft Bicycle Plan is adopted and implemented. Both pedestrian and bicycle modes of transportation can be supplemented by a levee trail along the Guadalupe River bypass channel.
2. Some vacant or surplus lands may be used to provide a new park or other open space opportunities (such as a community garden). This will require the creative use of all available open space opportunities.

Transportation Constraints

1. The amount and intensity of development may need to be limited if new development is determined to increase traffic to a level inconsistent with the City's transportation level-of-service policies.
2. The Tamien Multi-Modal Station has limited amenities (no open restrooms, refreshment concessions, or adequately covered platform) which could discourage transit use. Limited pedestrian amenities and the poor condition of the streetscape in the Tamien Station area could also discourage transit use as well as pedestrian activity in general.
3. The major transportation facilities in the Tamien Station area are also major noise generators and could expose new development to significant noise impacts.

Park and Open Space Constraints

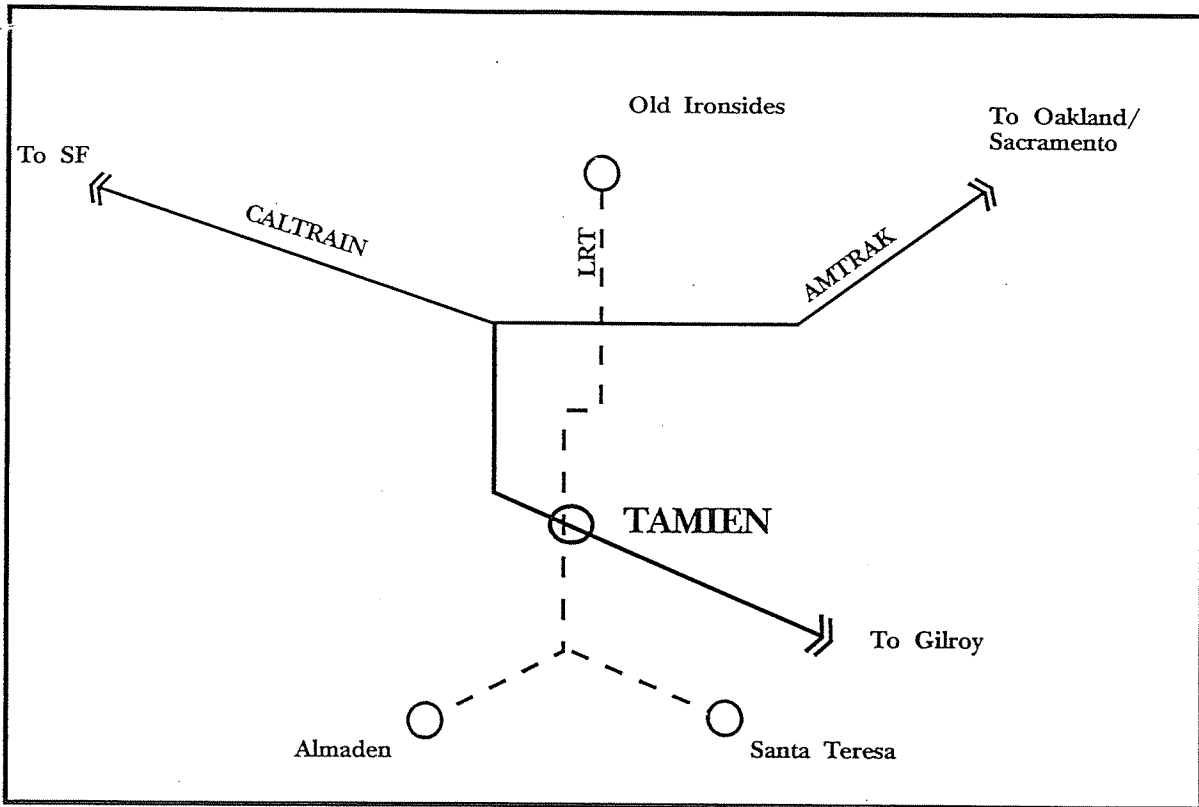
1. There is insufficient land available to meet all existing and projected open space needs.

Park and Open Space Opportunities

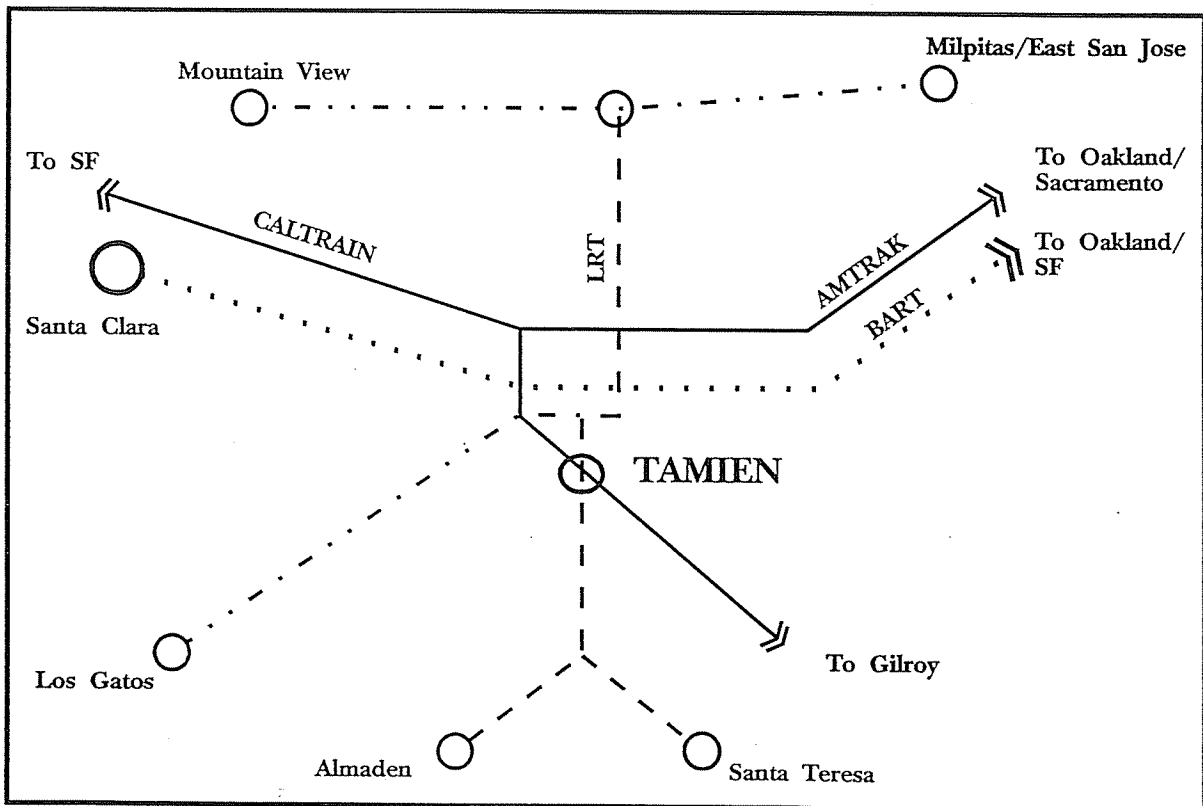
1. The Guadalupe River is the most

TRANSIT/RAIL SYSTEM SCHEMATIC

Figure 4



Existing Transit/Rail



Future Transit/Rail

2. There may be insufficient funding available to provide for the acquisition, development and maintenance of new park, recreation, and open space facilities.

Community Facilities and Utilities Opportunities

1. A day care facility is currently being constructed by the Santa Clara County Transit District on a portion of the CalTrain (eastern) parking lot for the Tamien Multi-Modal Station.
2. Residents of the Tamien Station area have access to a variety of community facilities, including libraries and parks, that are located in adjacent neighborhoods. Police and fire protection are available to the area and should be able to serve new development.
3. The Tamien Station area is currently served by major utilities such as storm sewers, sanitary sewers, water, electricity, gas and telephone. With some improvements, these existing utilities can adequately serve new development.

Community Facilities and Utilities Constraints

1. Existing community facilities in the vicinity, such as neighborhood parks, will be strained by new development since the capacity of these facilities is limited. The general area has already been identified as park deficient. Access to some existing parks in the vicinity, particularly by

younger children, may be difficult since some parks are not located within reasonable walking distance.

2. Funds are not available to provide new community facilities in the area.

Urban Design Opportunities

1. The Tamien Station area lends itself to creating a planned community which is compact, livable, and pedestrian oriented.
2. Existing neighborhoods and some significant structures in the vicinity can be used to create guidelines for new residential development. New residential development may draw upon various design elements (such as front porches) that characterize existing residential neighborhoods.

Urban Design Constraints

1. The need to protect existing residential neighborhoods from adverse visual and privacy impacts may limit the range of building configurations for new development.

GOALS AND OBJECTIVES

Introduction

The goals and objectives of the Tamien Station Area Specific Plan were developed by the Task Force and describe the key themes of the specific plan. Taken together, the goals and objectives provide a general vision of the desired future for the Tamien Station area. The purpose of the Plan is to make this vision a reality. The key to the Plan is to take advantage of the rare combination of a heavy rail facility serving the San Francisco Bay Region (CalTrain) and a light rail facility serving a large portion of Santa Clara County (the Guadalupe Corridor). Transit stations for both facilities are in close proximity to vacant and underutilized lands in the Tamien Station area.

The goals and objectives of the Plan are consistent with and support each other. No goal or objective stands alone and a complete vision of the Plan can only be achieved by considering all of its goals and objectives, and the policies designed to implement them.

Support Transit Use and Create a Pedestrian Friendly Environment

The Plan for Tamien seeks to create a combination of land uses that will support and encourage transit use and reduce automobile use. The key components in the proposed combination of land uses are the high density residential uses (including those in mixed use developments) located close to the Tamien Light Rail and CalTrain

Stations. Higher densities create a "ready market" of potential transit patrons who can use light rail or CalTrain to commute to major employment centers to the north. Light rail also provides access to shopping and entertainment in Downtown.

The Plan also strives to create a pedestrian friendly environment. Mixed use development in Tamien will provide retail uses and services close to new and existing residential development to allow residents to satisfy their daily shopping or service needs by simply walking downstairs or down the block. An attractive combination of retail and service uses and well designed pedestrian and street amenities will create a pleasant walking experience for both residents and transit users.

Transit and Pedestrian Goals and Objectives

1. Create a combination of land uses that effectively support transit use, reduce dependency on the automobile, and attract pedestrian activity.
2. Locate the most intense uses and highest residential densities on sites in close proximity to transit facilities.
3. Locate uses that will attract pedestrian activity or meet day-to-day shopping needs on the lower floors of the buildings containing either mixed uses or commercial uses.

4. Design the lower levels of buildings and walkways to create a secure, comfortable, and interesting pedestrian walking environment.
5. Encourage commercial and service uses to locate in the Tamien Station area to serve residents and transit patrons.
6. Replace existing land uses near transit facilities with more intensive land uses as market forces dictate or opportunities arise.
7. Reinforce the Willow Street Neighborhood Business District (NBD) by providing improved pedestrian linkages between existing and new residential development and the Willow Street NBD.

Livable Community

As a whole, the goals and objectives of the Plan propose the creation of a livable community that supports transit and the principles related to compact, pedestrian friendly development. In addition, the Plan seeks to fully utilize the existing and proposed amenities in the Tamien Station area. Examples include the Guadalupe River as a visual amenity, and the proposed four acre Transit Core Subarea park and the levee trail in the bypass channel as recreational amenities. The Plan also strives to protect residents and property from potential environmental hazards as well as to minimize damage to the natural environment from development.

Livable Community Goals and Objectives

1. Create a livable community that effectively utilizes the natural and man-made amenities of the Tamien Station area.
2. Build on the unique features of the Tamien Station area to create an image of Tamien as a special place.
3. Provide safe and convenient pedestrian access to link new and existing high density residential development to the Tamien Multi-Modal Station.
4. Encourage multiple use of the open space resources of the area.
5. Provide new park and recreation facilities that meet some local needs and create attractive focal points or gathering areas for the community.
6. Improve pedestrian walkways and the streetscape through a unified landscape-plan, public art, street furniture and other improvements that effectively attract pedestrian activity.
7. Provide trail facilities along or within the Guadalupe River and tie these facilities to other existing trails or pedestrian or bicycle routes.
8. Protect people and property in the Tamien Station area from potential environmental hazards such as noise, flooding, and the impacts associated with increased traffic.

Housing

The Plan creates opportunities for the City to increase its housing supply and use existing infrastructure and urban services more efficiently. These opportunities support the City's existing policies which encourage the provision of a variety of housing for all economic segments of the community in a compact, infill form. The Plan also provides some opportunity for the creation of more affordable housing given the high and very high residential densities proposed in the Plan. The Plan does not include specific affordable housing goals since it is expected that affordable housing units can be more successfully created, particularly as part of mixed income projects, through the flexibility incorporated into the Plan. The City, through Housing Department programs, has already provided some financial assistance for new housing projects in the Tamien Station Area and can be expected to do so in the future. Santa Clara County owns land in the area and may seek to provide specific affordable housing opportunities on these lands.

Housing Goals and Objectives

1. Provide a variety of housing opportunities that are suitable for a range of household incomes and sizes and housing types that support an efficient, compact form of development.
2. Provide opportunities for ownership and rental housing in the Tamien Station area.
3. Provide opportunities for affordable housing to be integrated into high

density residential, market rate projects consistent with the housing policies of the General Plan.

4. Explore methods to provide financial or other assistance for the construction of housing for very low, low, and moderate income households.

Neighborhood Compatibility

The design and treatment of new development will be important to the existing residential neighborhoods in the Tamien Station area. The Plan provides design policies that strive to ensure that the more intensive new development will be compatible with existing residential neighborhoods and will maintain their integrity. The setbacks, height, and massing of individual high density residential or mixed use buildings are carefully controlled by the Plan to promote interesting pedestrian environments as well as to protect the scale and "feel" of existing neighborhoods.

Neighborhood Compatibility Goals and Objectives

1. Maintain the positive qualities and characteristics of existing neighborhoods by ensuring that new development is compatible with existing neighborhoods.
2. Create a positive environment for the new neighborhoods developed in the Tamien Station area by preserving and enhancing the character of existing neighborhoods.

3. Design all new specific plan development to connect to existing surrounding residential and commercial areas via a strong network of streets, sidewalks, and other pathways.
4. Create a unique community and link the diverse parts of this community with each other and with surrounding areas by borrowing architectural styles, design elements, and landscape themes from existing adjacent neighborhoods.

LAND USE PLAN AND LAND USE DESIGNATIONS

The Land Use Plan (see Figure 5) for the Tamien Station Area Specific Plan establishes the nature and distribution of the land uses within the planning area. The land use designations listed below describe, in general, the appropriate land uses and amount of development intensity desired to achieve the objectives of the Specific Plan and to take advantage of two major transit facilities: the Tamien Light Rail Transit Station (which also provides important bus service links) and the Tamien CalTrain Station. The land use plan and its specific land use designations reflect the overall vision of the Tamien Station Area as a high density residential and mixed use neighborhood that effectively supports transit use and creates an interesting, livable, and secure environment. Most of the new land uses in the Tamien Station area will be high density residential or mixed uses generating approximately 1,225 dwelling units. New commercial development is expected to generate about 30,000 square feet of commercial space.

The land use designations of the Tamien Station Area Specific Plan are based on the land use designations found in the General Plan for the City of San Jose, but have been tailored to the Tamien Station area. Development within these designations is more specifically guided by the land use and design policies established for each of the three subareas identified in the Specific Plan: **Transit Core**, **Guadalupe River**, and **Almaden Road** (see Figure 6).

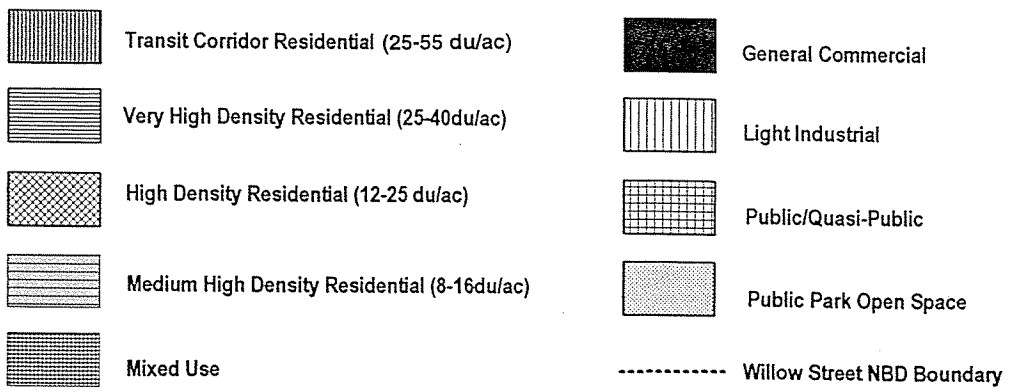
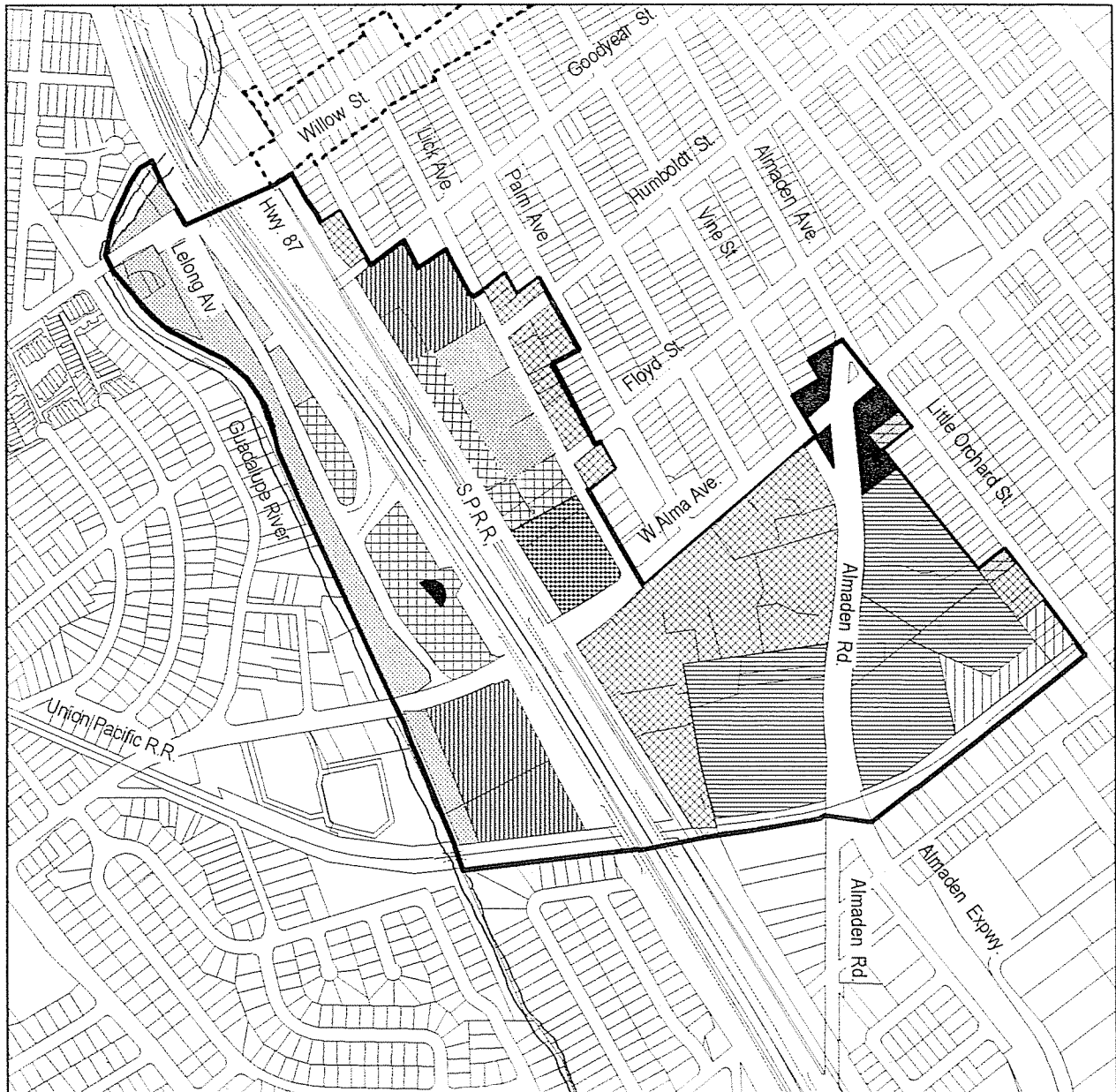
The land use designations for the Tamien Station Area Specific Plan are:

1. **Transit Corridor Residential: 25-55 Dwelling Units Per Acre (DU/AC)**

This land use designation is intended for high and very high density residential uses on lands close to the Tamien Light Rail Transit and CalTrain Stations. Residential development under this designation is expected to be oriented to transit and to encourage transit use. This designation is consistent with the urban form of the Transit Corridor Residential (TCR) land use designation described in the General Plan. This category applies to sites located within 2,000 feet of passenger rail stations in areas where intense development exists or is expected to exist. Development may be wholly residential or may be vertical mixed use with commercial service and/or office uses on the first two floors and high density residential above. The density range for this designation is 25-55 DU/AC, however, this Plan strongly encourages an average density of at least 35 DU/AC for each of the two major sites with this designation to take full advantage of the close proximity of these sites to the transit stations. Drive through uses are prohibited.

LAND USE PLAN

Figure 5



Scale: 1" = 750'

2. Very High Density Residential: 25-40 Dwelling Units Per Acre (DU/AC)

This land use designation is for those sites located further than 2,000 feet (walking distance) from the Tamien transit stations but which are suitable for high density residential uses. An average density of 30 DU/AC is strongly encouraged under this designation. This density is typified by three-story apartments or condominiums over parking.

3. High Density Residential: 12-25 Dwelling Units Per Acre (DU/AC)

This land use designation acts as a transitional land use category separating higher density residential development from lower density single-family neighborhoods. The relatively lower densities allowed under this designation are typified by two-story apartments or condominiums with surface parking that can be more easily designed to be compatible with adjacent single-family neighborhoods. Although the allowed density may be lower under this designation, significant numbers of new residents can be generated to support transit use, particularly if attractive pedestrian links are provided.

4. Medium High Density Residential: 8-16 Dwelling Units Per Acre (DU/AC)

This density is typified by patio homes, townhouses and duplexes

and is planned for a few small lots on Little Orchard Street that are adjacent to or facing existing single-family or duplex dwellings and that are too small to be efficiently developed with high density residential. This designation allows a mixture of single-family and/or multi-family dwelling units subject to overall density limits.

5. Mixed Use

This land use designation allows a combination of high density residential and commercial uses in a vertical configuration (residential above commercial and/or parking). Industrial uses are not permitted under this designation. Parking components can be private or public.

Sites with this designation should be developed primarily in high density residential use within the 25-55 DU/AC density range similar to the Transit Corridor Residential (TCR) designation. As in the TCR designation, this Plan strongly encourages an average density of at least 35 DU/AC on the major site designated for Mixed Use.

Commercial uses should also be provided under this designation to serve on-site residents as well as residents in nearby neighborhoods and transit patrons. Allowed commercial uses are neighborhood serving retail, service and/or office uses to provide goods and services for residents, transit riders, and others. Allowed commercial uses may also include community serving

uses, such as day care and telecommuting facilities.

Commercial uses should not be contained in freestanding buildings but may be located on the first two floors of a vertical mixed use building. Drive-through uses are prohibited.

6. General Commercial

This non-specialized commercial designation is intended to permit a variety of commercial uses oriented to serve the needs of transit riders, residents or customers generated by traffic along major streets and also to provide jobs. Retail and service commercial and business and professional office uses are allowed under this designation.

Examples of permitted retail and service commercial uses include: 1) basic convenience shopping and services for local residents, such as small grocery/deli, bakery, cleaners, video outlet, beauty shop, ATM machine or branch bank and postal/business services; 2) food and beverage outlets, including neighborhood serving cafes and bars; 3) specialty convenience stores, such as flower shop, card/office supply store, gift shop and book store. Drive through uses are prohibited.

7. Light Industrial

The Light Industrial designation is intended for industrial uses that exclude uses with unmitigated hazardous or nuisance effects. A

very limited portion of the Tamien Station Area is designated for Light Industrial uses and its primary purpose is to recognize that some existing industrial uses, such as Stucco Supply, are expected to be in operation for the long term. If in the future, however, the property owners wish to discontinue the existing industrial uses, the City should consider replanning areas with this designation for residential use.

8. Public/Quasi-Public

This category is used to designate public land uses such as the light rail station parking lots and the County day care facility. The uses under this designation are designed primarily to support transit use.

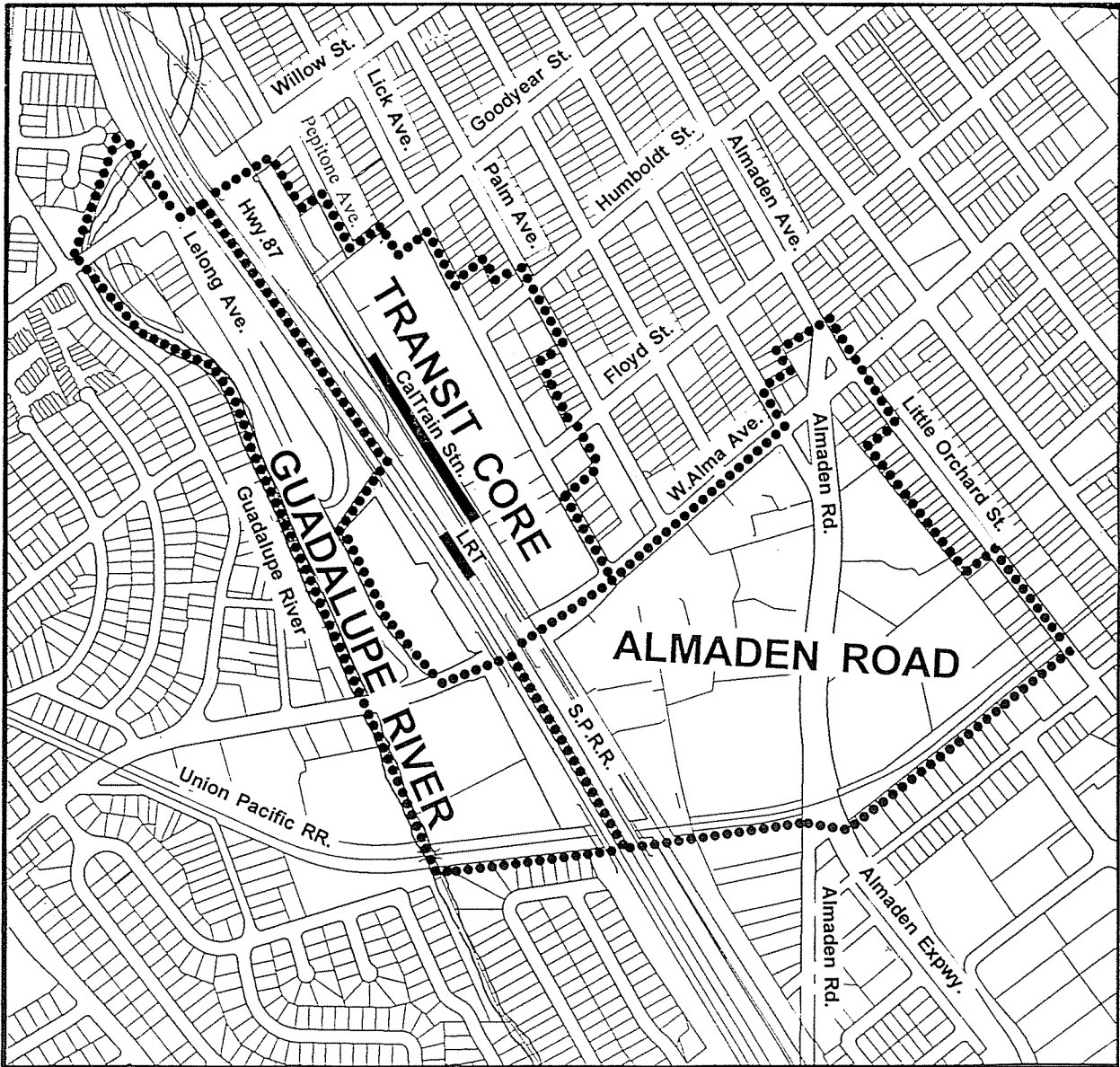
This designation also allows small retail commercial kiosks or other very small structures no larger than 1,000 square feet to provide retail services to transit patrons and the community. These structures are limited to an aggregate total of 2,000 square feet in the Plan area.

9. Public Park/Open Space

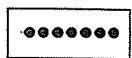
This designation is applied to lands which are publicly owned and are intended for open space and recreational use. Much of the open space in the Tamien Station area is contained in the Guadalupe River Bypass Channel area which may provide limited recreational opportunities with the development

SUBAREAS

Figure 6



Tamien LRT and CalTrain Stations



Subareas Boundary



Scale: 1"=750'

of the Guadalupe River Trail. This designation is applied only to lands owned by public agencies or programmed for acquisition.

This designation also allows small retail commercial kiosks or other very small structures no larger than 1,000 square feet to provide retail services to transit patrons and the community. These structures are limited to an aggregate total of 2,000 square feet in the entire Plan area.

LAND USE AND DESIGN POLICIES

TRANSIT CORE SUBAREA

General. The Transit Core Subarea provides the greatest opportunity to create, intensive high density residential or mixed use development designed to support transit use in the Tamien Station area since both the Tamien CalTrain and Light Rail Stations are located in this subarea. This is the key activity center of the Tamien Station area with the transit stations acting as the hub. The success of the Tamien Station Area Specific Plan will largely depend on the successful development or redevelopment of this subarea.

Subarea Boundaries. The Transit Core Subarea is bounded on the south by Alma Avenue, on the east by Lick Avenue or Palm Avenue, on the north by Goodyear Street or Willow Street and on the west by State Route 87 and that portion of Lelong Avenue and the State Route 87 on-ramp adjacent to the main light rail parking lot.

Residential Development. There are two major opportunities for high density residential development or redevelopment in this subarea. The Santa Clara County Transportation Agency (SCCTA) parcel (about 9.5 acres) is located south of Goodyear Street between Lick Avenue and State Route 87. The northernmost 6 acres of this parcel are vacant. The remainder is used for the CalTrain parking lot. A children's day care facility will be constructed on a 0.7 acre portion of the southeasterly corner of the parking lot to

serve transit patrons and local residents. The second major opportunity is the Alma Bowl/Sprig Electric Company site (about 2.7 acres) bounded by the Tamien CalTrain Station/Route 87 on the west, Alma and Lick Avenues on the south and east, and the SCCTA parcel on the north.

Vertical Mixed Use Commercial. In addition to the high density residential described above, the mixed use designation of the Alma Bowl/Sprig Electric site allows commercial uses in a vertical mixed use configuration. In the Transit Core Subarea, neighborhood serving commercial uses will be an important component of vertical mixed use development. The population generated by new high density residential uses, as well as existing residents and transit riders, will need support commercial uses. The commercial uses to be located in this area should be small retail shops, restaurants, services, or offices primarily designed to serve this population, but could also be oriented to attract potential customers generated by traffic on Alma Avenue.

Commercial Development. An opportunity exists to allow a limited amount of general commercial development in the parking lot on the west side of Tamien Station. The specific plan allows freestanding commercial development oriented primarily to pedestrians, bicyclists, and transit riders and, secondarily, convenient for passing motorists. The development should be designed to be visible and accessible from both the Tamien LRT Station and Lelong Avenue.

Park and Community Facilities. This subarea features a four acre park that can act as a focal point for community activities and provide open space for existing and new residents as well as transit users. In addition to the park, the day care facility to be provided on the Santa Clara County Transportation Agency site will serve both residents and transit riders.

Industrial Development. No new industrial development is envisioned in the Transit Core Subarea but existing industrial uses are expected to remain in the interim.

Landmark. A tall clock tower or similar structure is proposed near the entrance to the CalTrain Station to create a community focal point and to establish an image that will become associated with the area and provide it identity.

LAND USE POLICIES

1. **Housing Variety.** A range of high density housing types suitable for a variety of household sizes is encouraged. A mix of rental and ownership housing is also encouraged.
2. **Transit Corridor Residential.** New residential development on land designated Transit Corridor Residential should occur within a density range of 25-55 DU/AC. An average density of 35 DU/AC is strongly encouraged. A small amount (no more than 5,000 square feet) of neighborhood serving commercial space may be located on the two lowest floors.
3. **Existing Uses.** Existing industrial, commercial and residential uses are allowed to continue in the interim; the sites occupied by these uses represent valuable housing opportunities, however, and should eventually be converted to high density residential or mixed use.
4. **Mixed Use.** Those areas designated for mixed use should be developed with a combination of high density residential, commercial, office and/or parking uses in a vertical mixed use configuration (e.g., residential above commercial). Residential densities should range between 25-55 DU/AC. An average density of 35 DU/AC is strongly encouraged. To ensure that adequate commercial uses are available to serve the vicinity, a minimum of 5,000 square feet of neighborhood serving commercial space should be provided on the Alma Bowl/Sprig Electric site. A maximum of 20,000 square feet of commercial space is allowed for the area designated mixed use.
5. **Kiosks/Small Commercial Structures.** Commercial development in the Transit Core Subarea should occur in a vertical mixed use configuration, except that several small (i.e., no larger than 1,000 square feet) kiosk-type structures or small commercial structures, totalling no more than 2,000 square feet in the aggregate, may be allowed to house commercial uses oriented to transit patrons and area residents, such as coffee shops. The kiosks should be located near

the CalTrain Station, its vicinity or within the CalTrain Station and should be oriented to the station's entrance or the public park. The kiosk or kiosks could be located on those portions of the SCCTA site designated Public Park/Open Space, or Public/Quasi-Public. Drive through uses are prohibited.

6. **LRT Commercial.** A maximum of 5,000 square feet of commercial uses may be developed in a single area within the Tamien Station LRT parking lot. Commercial uses should not interfere with the normal operation of the multi-modal station.
7. **Willow Street Neighborhood Business District.** Commercial land uses in this subarea are intended to complement the commercial uses located in the Willow Street Neighborhood Business District to the north.

DESIGN POLICIES

The following design policies are intended to create an attractive living environment, to ensure that new development will be designed to be compatible with existing neighborhoods, and to avoid impairing the viability or character of existing neighborhoods. These design policies are consistent with achieving the average densities envisioned in this plan.

For purposes of project design, compatibility does not require that the massing of new development should be the same as existing

buildings but that the massing of new buildings should not be significantly different from that of existing buildings at the interface. New residential development should draw on common architectural elements, such as front porches, and materials that are characteristic of the existing neighborhood to better blend in with the existing neighborhood. New development should also be designed to create a secure living environment for both new and existing residents.

The design policies for the Transit Core Subarea are also intended to create an attractive pedestrian environment to draw patrons to the light rail and CalTrain stations. New residential buildings should have a strong street presence and orientation. New buildings near the public park should also be oriented to the park and reinforce its identity as a public space serving the community. The park itself should also serve as the grand entry for the Tamien transit facilities.

General

1. **Residential Design Guidelines.** New residential development should conform with the Residential Design Guidelines of the City of San Jose unless otherwise stipulated in the following policies.
2. **Security.** Residential and mixed use development should be designed to enhance the security of residents by avoiding potential hiding places, by providing adequate light (both on-site and off-site), and by creating space that is actively used and observable.

Building Orientation and Design

3. **Building Orientation to Streets and Park.** Residential development within the Transit Core Subarea should have a strong "street presence" toward Lick Street and Palm Avenue and also orient to the proposed public park. Buildings should have front entries oriented towards these streets or the park. Parking, parking structures or the rear of buildings should not face Lick or Palm Avenues or the park.
4. **Building Design.** Building design should promote a visually diverse and rich fabric, complement the existing neighborhood scale, and reinforce the pedestrian orientation of the Tamien Station area. To help larger structures maintain a human scale typical of smaller buildings and to create pedestrian interest, architectural treatments of building facades and roofs should include sufficient articulation and variation to create an impression of individual units or clusters of units compatible with the existing neighborhood and an active pedestrian environment. The use of sloping roofs, decorated parapets, varying floor planes, different but related ornamental elements, balconies, porches, bay windows and other architectural elements is encouraged to achieve variety and articulation. All mechanical equipment should be screened from public view.
5. **Pattern of Development.** A pattern of development and street improvements should be established that reinforces and extends the pedestrian character of adjacent areas and enhances linkages to the Willow Street Neighborhood Business District and Alma Avenue. This can be accomplished through frequent spacing and orientation of building entries, incorporation of porches, improved street landscaping and lighting, interesting building design and materials, and other suitable improvements.
6. **Residential Entries.** The primary entries for high density residential development should be oriented to the street or the public park at frequencies no greater than 50 feet. "Ground level" units should be at least three feet and no more than six feet above grade.
7. **Commercial Orientation.** Commercial uses should be oriented to Lick and Alma Avenues. These uses may also be oriented to the CalTrain and Day Care Center parking lot. Due to the relatively steep slope within the Alma Avenue right-of-way, design elements associated with commercial orientation (i.e., windows and entries) need not extend more than 150 feet westerly of Lick Avenue. Commercial entries facing Lick or Alma Avenues should be no more than 50 feet apart. Stairs and ramps may be used to provide access to commercial uses along this portion of Alma Avenue.

8. **Commercial Design.** Ground floor commercial space should be designed as an integral part of a mixed-use building, but should also be designed as a distinct part of the building facade, with materials and treatments that offer visual interest to the pedestrian. Clear glass display windows, awnings, outdoor dining areas, and/or landscaping planters should be used to create attractive entryways and encourage street activity.

9. **Commercial Orientation - Tamien LRT Station.** New commercial buildings in the Tamien LRT Station parking lot should be oriented to both Tamien Station and Lelong Street. Entrances and/or store fronts should be visible from both Tamien Station and Lelong Avenue.

10. **Commercial Design - Tamien LRT Station.** New commercial development in the Tamien LRT Station parking lot should be designed:

- To consist of freestanding buildings oriented to pedestrians;
- With convenient access from all areas of Tamien Station via a network of pedestrian paths separate from vehicular circulation and consistent with the circulation policies of this plan.
- For convenient access to the park and ride lot. Parking spaces at a rate of one space

per 1,000 square feet of net space should be in some way reserved for commercial use; and,

- To avoid conflict with the normal operations of the multi-modal station such as bus and automobile circulation.

Massing and Height

11. **Maximum Building Heights - West Side of Lick Avenue.** Buildings on parcels located to the west of Lick Avenue (excluding those facing Pepitone Avenue) are allowed a maximum height of 65 feet; to encourage visual diversity, however, this height may be increased to 90 feet and limited to a few building elements located at least 200 feet from any single-family neighborhood. Development height should be limited to 25 feet within 25 feet of a property line adjacent to or opposite (i.e., across the street from) a property planned and developed with lower density residential uses such as single-family dwellings or duplexes. Development height may gradually increase one foot for every foot of horizontal distance beyond the original 25 feet up to a maximum of 65 feet (see Figure 7).

12. **Privacy and Shade.** Structures taller than 30 feet should be designed to avoid significant privacy and shade impacts on adjacent single-family or duplex neighborhoods. No windows in these taller structures should have a direct line of sight into any single-family or duplex rear yard.
13. **Maximum Building Heights - Easterly of Lick Avenue and West Side of Pepitone.** Residential development on parcels located easterly of Lick Avenue or on the west side of Pepitone Avenue should be limited to two-stories and 35 feet in height. The scale and design of this development should be generally consistent and compatible with the existing neighborhood.

TRANSIT CORE SUBAREA
Key Development Standards

Maximum Height	
Parcels west of Lick Avenue (excluding those facing Pepitone)	65 feet*
Parcels easterly of Lick Avenue or the west side of Pepitone Avenue	35 feet; 2 stories*
Landmark/Limited Building Elements	90 feet*
Setbacks	
<i>Residential</i>	
From Southern Pacific Railroad	100 feet*
From Lick Avenue	Min. 15 feet; max. 25 feet
From Alma Avenue	Min. 5 feet and max. 20 feet from the top of slope of the adjacent right-of-way
Porches, decks, and stairways	May encroach into front setback.*
All other setbacks	Residential Design Guidelines
<i>Commercial</i>	
From Lick Avenue	Max. 5 feet*
From Alma Avenue	As small as possible consistent with the need to address the front slope.
All other setbacks	Commercial Design Guidelines
Parking	
Residential and Mixed-use	San Jose Zoning Ordinance*
Commercial @ Tamien LRT	1 space/1,000 square feet of commercial space.

* Refer to policies for additional limitations or exceptions.

Development height should be limited to 25 feet within 25 feet of a property line adjacent to or opposite (i.e., across the street from) a property planned and developed with lower density residential uses such as single-family dwellings or duplexes. Development height may gradually increase one foot for every foot of horizontal distance beyond the original 25 feet up to a maximum of 35 feet.

14. **Maximum Building Height - Tamien LRT Station.** New buildings located in the Tamien LRT Station parking lot should maintain a maximum height of 35 feet.

Setbacks and Buffers

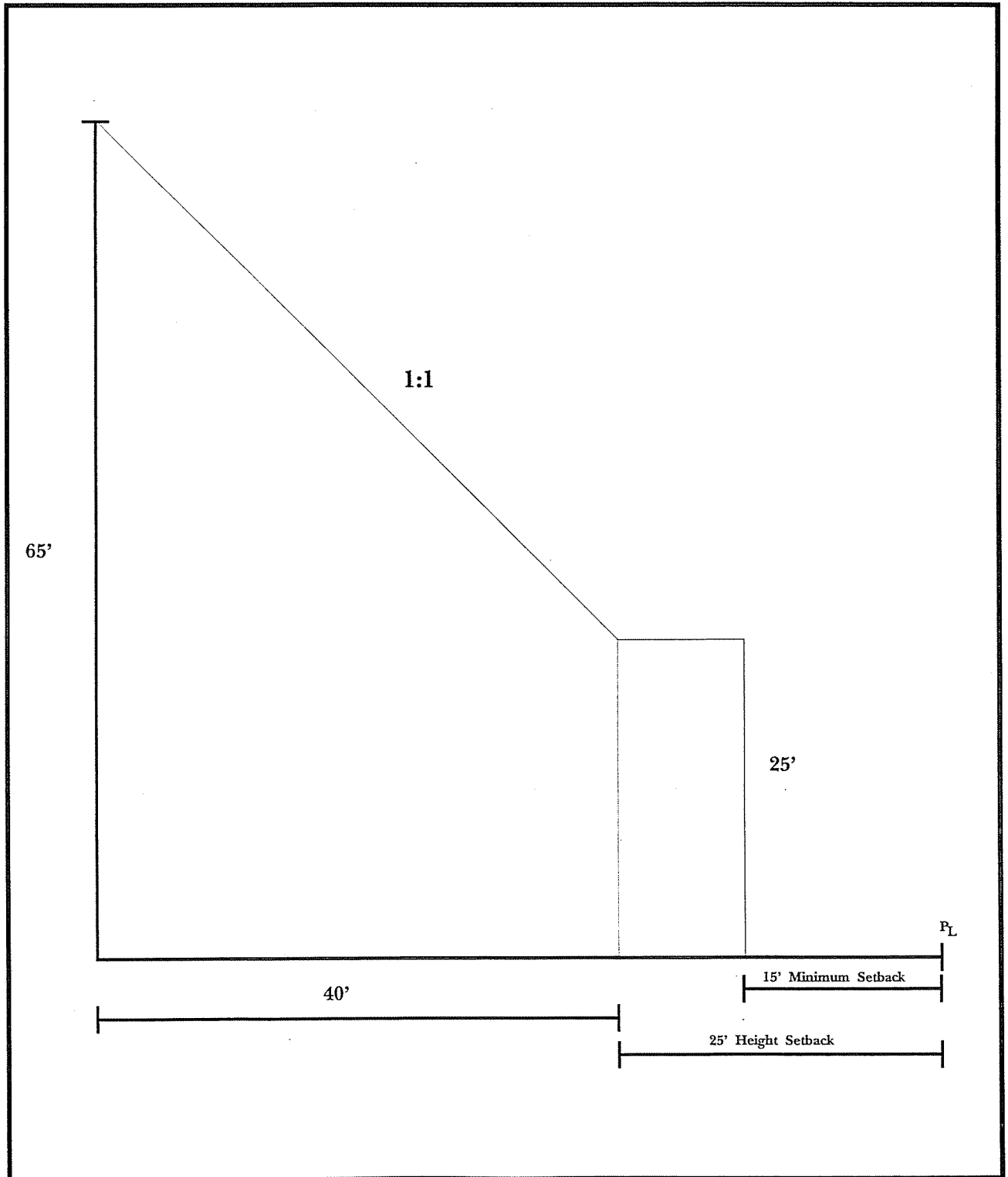
15. **Residential Setback From Southern Pacific Railroad.** To avoid the adverse impacts associated with noise and vibration, new dwelling units on the Santa Clara County Transportation Agency (SCCTA) parcel and Alma Bowl/Sprig Electric site should be set back a minimum of 100 feet from the Southern Pacific Railroad tracks. This area could be used for driveways, parking and pedestrian walkways. This setback may be reduced if equivalent noise and vibration mitigation can be provided by other means. Screening treatments including an eight-foot tall, articulated wall, with adjacent tall and densely planted trees, should be provided parallel with the railroad tracks. If this wall also needs to

function as a soundwall, its design should conform to the noise reduction provisions of any required noise study.

16. **Residential Setback From Lick and Alma Avenue.** Residential structures developed along Lick Avenue should maintain a setback of no less than 15 feet and no more than 25 feet. Residential structures developed along Alma Avenue should maintain a setback between 5 and 20 feet from the top of slope of the adjacent right-of-way. Porches, decks, and stairways may encroach into this setback up to a maximum of 60% of the total setback area. Setback areas should be well landscaped with low hedges, trees, and flowering shrubs that create diversity and interest along the street.
17. **Commercial Setback.** To encourage pedestrian activity, commercial/mixed use development along Lick Avenue should maintain a front setback of no more than 5 feet unless a somewhat larger setback is necessary to accommodate outdoor dining. Commercial setbacks along Alma Avenue should be as small as possible consistent with the need to address the front slope.

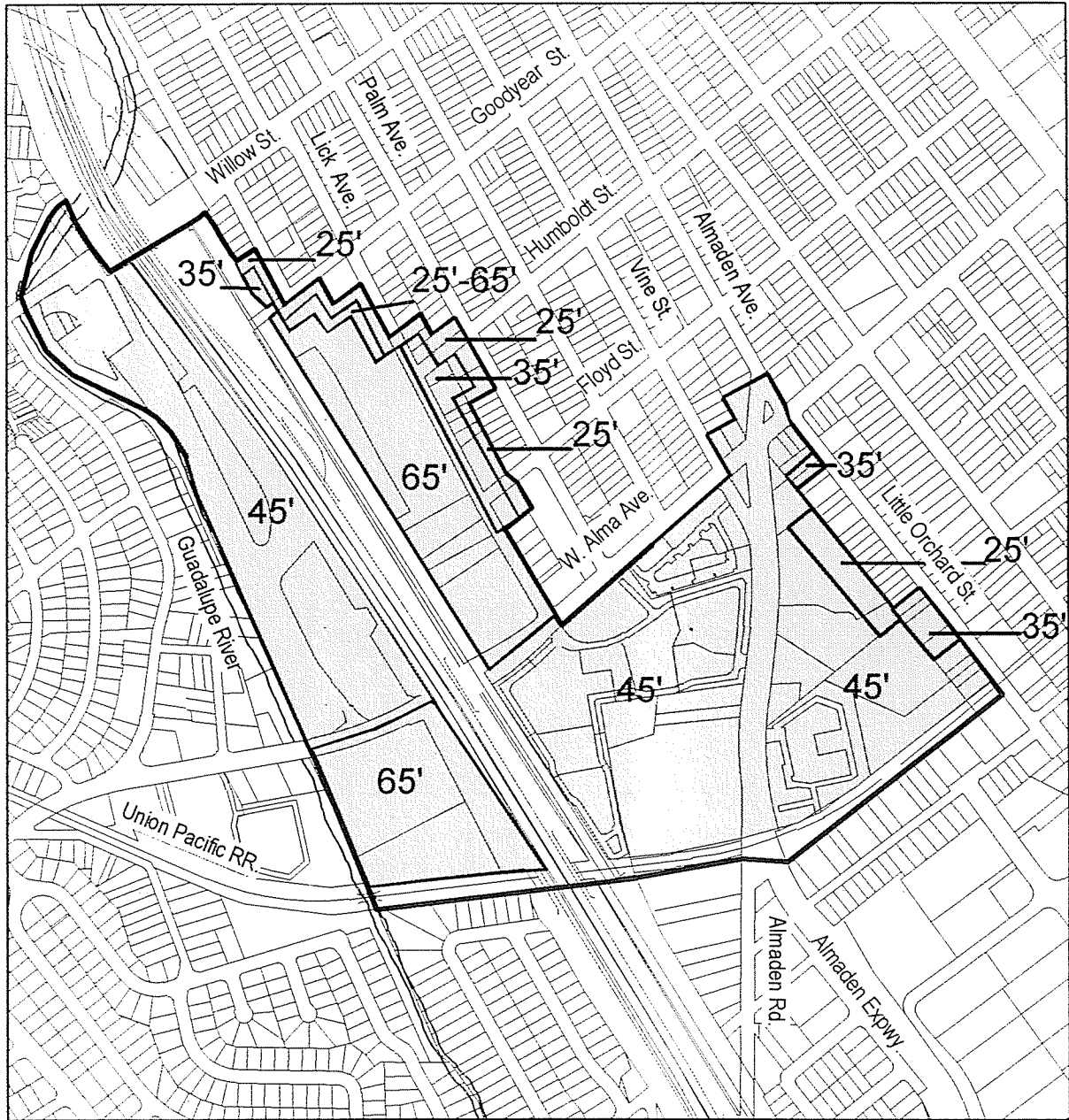
Parking and Servicing

18. **Parking Standards.** Parking standards for new residential and mixed use development in the Transit Core Subarea should comply



MAXIMUM HEIGHT LIMITS

Figure 8



Scale: 1"=750'

with the parking requirements of San Jose's Zoning Ordinance. Since the Transit Core Subarea contains major transit facilities, parking reductions may be permitted if supplemental parking need studies warrant such a reduction based on transit ridership, pedestrian use of commercial facilities, shared parking, or other appropriate factors.

19. **Commercial - Tamien LRT Station.** In general, the short term park and ride lot will provide parking for commercial development in the Tamien Station parking lot. One parking space per 1,000 square feet of net commercial space should be provided in close proximity to the commercial building and reserved for commercial use.
20. **Treatment of Parking Facilities.** Parking facilities should not be generally visible from Alma Avenue, Lick Avenue, the CalTrain entry drive, or the park. Driveway curb cuts and garage entries should be few to avoid detracting from the pedestrian environment. Parking stories located beneath buildings should be depressed and should not rise above grade more than four feet. Porches, stairways, and other architectural treatments should be used to help screen parking story openings other than those necessary for driveways. Landscaping may be used to supplement the screening provided by architectural treatments. Parking may be located at or above grade if the parking area will be encapsulated by ground-level

commercial or residential uses. Because blank walls tend to depress pedestrian environments, blank walls more than four feet high should not be used to screen parking areas from the street.

21. **Service Facilities Screening.** Truck loading docks should be designed as an integral part of mixed use structures and should not be oriented to any public right-of-way or open space amenity. All garbage and refuse facilities and mechanical equipment should be included within buildings or screened from public view in a manner compatible with the overall building design and streetscape treatment of the development.

Open Space and Landscaping

22. **On-Site Open Space.** A system of attractive, on-site open spaces and common areas should be integrated into all new residential and mixed use projects to create comfortable areas for people to meet, interact and enjoy open space amenities. Usable common open space and private outdoor open space should, at a minimum, be provided at levels required in the Residential Design Guidelines. Common open space may be reduced on site if an equivalent area is dedicated and improved as public park.
23. **Public Park.** The park proposed on the Santa Clara County Transportation Agency (SCCTA) site should be four acres in size. The

park should be compact, generally rectilinear in form and designed to be clearly accessible and usable by the public. The SCCTA may dedicate and improve a portion of the land for this park in lieu of meeting the common open space standards described in the Residential Design Guidelines that would apply to residential development on the SCCTA site. The location of the park may vary from the location shown on the Tamien Land Use Plan to allow for some flexibility in future development planning but it should remain in the same general area. The location of the proposed park should be established through the preparation of a master development plan for the entire SCCTA site. The master development plan may also determine the timing for the acquisition and development of the park commensurate with the phasing of residential development within the Plan area. In general, however, the first two acres of the park should be acquired and developed with the initial residential development of the SCCTA site. The second two acres should be acquired and developed after a certain number of new dwelling units

are constructed. In the interim, this second two acre portion should remain as the CalTrain parking lot and City staff will work with SCCTA and CalTrain staff to determine how the parking lot may be used for recreational purposes, such as basketball, during off-peak parking demand times. The master development plan should also identify a new location and/or configuration for the CalTrain parking lot to replace the portion of the parking lot to be occupied by the second phase of park development.

24. **Tamien LRT Station Platform and Landscaping.** A second shelter with landscaping should be provided on the Tamien LRT platform. Landscaping around the entrance to the Tamien LRT Station should be enhanced using sufficient trees, shrubs, flowers, etc. to create an attractive entryway. Bare or weedy embankment slopes should be planted with drought tolerant trees, shrubs, and groundcovers. The City should work with the Santa Clara County Transportation Agency to explore ways and means to provide the funding resources necessary to upgrade station landscaping and provide platform improvements.
25. **Landscape Plans.** Landscape plans and landscaping for public and private development should conform

to the City of San Jose Landscape and Irrigation Guidelines. Tree species used in landscaping should be similar to those found in adjacent neighborhoods.

Landmark

26. **Clock Tower.** A very tall architectural element, such as a clock tower, should be installed near the public park and the entrance to the CalTrain Station to provide a landmark and focal point for the Tamien Station area. This architectural element should not exceed 90 feet in height. The commercial space planned for the freestanding kiosk(s) may be incorporated into the bottom level of the clock tower or similar architectural element.

GUADALUPE RIVER SUBAREA

Subarea Boundaries. The Guadalupe River subarea is bounded by the Guadalupe River on the west, Willow Street on the north, Highway 87 and the Tamien LRT Station parking lot on the east, and the Union Pacific Railroad on the south. Route 87 dominates the eastern length of the subarea with its elevated structure and southbound Alma Avenue offramp to Lelong Avenue.

Residential Development. The 9.15-acre Elks Club property provides an excellent opportunity for high density residential development since the site is within convenient walking distance of Tamien LRT Station just across Alma Avenue. The site is located south of Alma Avenue, between Route 87 and the Guadalupe River, and north of the Union Pacific Railroad.

The specific plan allows the Elks Club to operate on this site indefinitely but also allows the existing lodge facility to be replaced by a smaller lodge facility with or without commercial uses. The remainder of the site would be developed with high density residential use. The entire site may also be converted to high density residential use.

New residential development on the Elks Club site should be oriented to the Guadalupe River and its bypass channel. Residents should be able to take advantage of the open space and trail opportunities that will be provided along the Guadalupe River. Landscaping and pedestrian trails for the Elks Club site can be integrated into or complement the riparian corridor trail system.

Open Space. The majority of the open space in the Tamien Station area is located along the Guadalupe River which is a scenic amenity as well as an important riparian habitat. The Guadalupe River is a key amenity which should be considered in the orientation and design of new development. Recreational opportunities along the river will be limited to trails and passive open space use due to the narrowness of the channel and the need for riparian mitigation planting.

A small community garden, however, is proposed on remnant property owned by the Santa Clara County Transportation Agency (SCCTA) on the east side of Lelong Street north of a small parking lot. Since limited open space is available in the Tamien Station area, the proposed community garden could provide a valuable opportunity for residents in high density projects to grow vegetables and flowers while enjoying the "outdoors".

LAND USE POLICIES

1. **Housing Variety.** A range of high density housing types suitable for a variety of household sizes is encouraged. A mix of rental and ownership housing is also encouraged.
2. **Transit Corridor Residential - Elks Club.** New residential development on lands designated Transit Corridor Residential should occur within a density range of 25-55 DU/AC. An average density of 35 DU/AC is strongly encouraged on this site. Commercial or office uses

may be allowed on lands with this designation on the lower floors of a primarily high density residential development or in a freestanding building if these uses are combined with a new lodge facility. The preferred arrangement, however, is to combine residential and non-residential uses in a vertical configuration. Commercial uses may occupy up to 10,000 square feet of building space in the aggregate.

3. **Community Garden.** When demand warrants and funding is available, a community garden should be created on the east side of Lelong Street adjacent to the Tamien Station parking lot north of the Route 87 offramp to provide an opportunity for area residents, particularly new, high density residents to enjoy gardening. The City should work with the community and the Santa Clara County Transportation Agency to establish the area and operational limits for a community garden on this site.
4. **Elks Club Lodge Facility.** A smaller lodge facility than that existing may be allowed on the Elks Club site as part of the reuse of the site for high density residential uses. The lodge facility may be located in a freestanding building facing Alma Avenue and may also include up to 10,000 sq. ft. of commercial space.

DESIGN POLICIES

The Guadalupe River Subarea is a gateway to the Tamien Station area via Alma Avenue and new development in this subarea will play an important role in establishing the image of the entire plan area. As with the other subareas, one of the purposes of the following design policies is to ensure that new development will be compatible with existing neighborhoods. The Guadalupe River Subarea is unique in that only one site, the Elks Club site, has any potential for new development. This site is somewhat isolated from other neighborhoods by a series of barriers: the Guadalupe River, the elevated Union Pacific Railroad, and State Route 87. Compatibility is still important in this case but would relate primarily to the character and scale of adjacent, existing streetscapes. These design policies are consistent with achieving the average densities envisioned in this Plan.

A key feature of the Elks Club site is its proximity to the Guadalupe River and the proposed flood control bypass channel. These features should be integrated into the site design of any new development and should affect the orientation of building entries and windows. Site design should also create the sense of "drawing" the riparian corridor into the project and provide the prime focal point for any residential development.

In general, the major design objectives for the Guadalupe River Subarea include: linking this area to a larger, active community that is attractive and that encourages people to interact with each other; taking maximum advantage of the open space and recreational opportunities of

the Guadalupe River Riparian Corridor; ensuring that development respects and preserves the natural environment; and, ensuring that the character of nearby neighborhoods is preserved.

General

1. **Residential Design Guidelines.** New residential development should conform with the Residential Design Guidelines of the City of San Jose unless otherwise stipulated in the following policies.
2. **Security.** Residential development should be designed to enhance the security of residents by not creating hiding places, by providing adequate light (both on-site and off-site), and by creating space that is actively used and observable.
3. **Commercial Design Guidelines.** New commercial development should conform with the Commercial Design Guidelines of the City of San Jose unless otherwise stipulated in the following policies.
4. **Riparian Corridor Development Guidelines.** All new development adjacent to the Guadalupe River or its bypass channel should be consistent with the provisions of the Riparian Corridor Development Guidelines contained in the City of San Jose's Riparian Corridor Policy Study.

Building Orientation and Design

5. **Commercial Orientation - Elks Club.** Commercial uses, either in a freestanding building combined with a lodge facility or in the lower levels of a primarily high density residential development, should be oriented towards Alma Avenue. Commercial entries should face Alma Avenue and be no more than 50 ft. apart.
6. **Commercial Design - Elks Club.** Ground floor commercial space should be designed as an integral part of the building, but should also be designed as a distinct part of the building facade, with materials and treatments that offer visual interest to pedestrians. Clear glass display windows, awnings, outdoor dining areas, and landscaping planters should be used to create attractive entryways and encourage street activity.
7. **Residential Design - Elks Club.** To help larger structures maintain a human scale typical of smaller buildings and to create pedestrian interest, architectural treatments of building facades and roofs should include sufficient articulation and variation to create an impression of individual units or clusters of units compatible with the existing neighborhood and an active pedestrian environment. The use of sloping roofs, decorated parapets, varying floor planes, different but related ornamental elements, balconies, porches, bay windows and

other architectural elements is encouraged to achieve variety and articulation.

8. **Other Design - Elks Club.** New residential and non-residential buildings on the Elks Club site should:

- Contain entrances and windows that are visible from the riparian corridor;
- Utilize landscaped, semi-public space along the riparian corridor to create transition areas between the public and private areas associated with residential development;

- Integrate landscaped buffers and soundwalls along interfaces with Route 87 and the Union Pacific Railroad; and
- Create a network of pedestrian paths to Alma Street and the Guadalupe River.

Height

9. **Residential and Commercial Height Limits.** New residential development should not exceed 65 feet in height in the Guadalupe River subarea. Residential development within 80 feet of the western property line or bypass channel

GUADALUPE RIVER SUBAREA
Key Development Standards

Height	
Residential	65 feet
All Other	45 feet
Setbacks	
<i>Residential</i>	
From Union Pacific Railroad	100 feet*
From Alma Avenue	15 feet
Porches, decks, and stairways	May encroach into front setback.
All other setbacks	Residential Design Guidelines and Riparian Corridor Development Guidelines
<i>Commercial or Lodge Facilities</i>	
From Alma Avenue	Max. 15 feet.
All other setbacks	Commercial Design Guidelines
Parking	
Residential and Non-Residential	San Jose Zoning Ordinance*

* Refer to policies for additional limitations or exceptions.

easement (whichever creates the greatest separation between buildings and the river) should not exceed 45 feet in height. All other development should maintain a maximum building height of 45 feet.

Setbacks and Buffers

- 10. **Commercial or Lodge Setback from Alma Avenue - Elks Club.** To encourage pedestrian activity, commercial or lodge uses facing Alma Avenue, in either a freestanding or vertical mixed use configuration, should maintain a front setback of no more than 15 feet.

- 11. **Residential Setbacks - Elks Club.** If the Elks Club site is to be developed wholly with residential uses, the minimum setback from Alma Avenue should be 15 feet. For no more than 40% of the length of the western property line or flood control easement (whichever is shorter), all structures should be setback at least 30 feet; for the remaining 60% of the length of the property line or easement, structures should be setback at least 50 feet. All other setbacks must be consistent with the Residential Design Guidelines and the Riparian Corridor Development Guidelines. Porches, decks, and stairways may encroach into the Alma Avenue setback up to a maximum of 60% of the total setback area. This setback area should be well landscaped with low hedges, trees, and flowering shrubs

that create diversity and interest along the street.

- 12. **Noise and Vibration Buffer.** To avoid the adverse impacts associated with noise and vibration, new dwelling units on the Elks Club site should be setback a minimum of 100 feet from the Union Pacific Railroad tracks. This area of the site could be utilized for parking or driveways. These setbacks may be reduced if appropriate noise and vibration reduction measures can be implemented which provide equivalent mitigation.

Parking and Servicing

- 13. **Elks Club.** The parking standard for residential and non-residential buildings on the Elks Club property should conform to the City of San Jose Zoning Ordinance. Since the Elks Club site is directly across the street from the LRT station, parking reductions may be permitted if supplemental parking need studies warrant such a reduction based on transit ridership, pedestrian use of commercial facilities, shared parking, or other appropriate factors.

- 14. **Treatment of Parking Facilities.** Parking facilities should not be generally visible from Alma Avenue. Driveway curb cuts and garage entries should be few to avoid detracting from the pedestrian environment. Parking stories located beneath buildings should be depressed and should not rise above

grade more than four feet; all openings should be screened using architectural treatments and landscaping. Parking may be located at or above grade if the parking area will be encapsulated by ground-level commercial or residential uses. Because blank walls tend to depress pedestrian environments, blank walls more than four feet high should not be used to screen parking areas from the street.

15. **Service Facilities Screening.** All garbage and refuse facilities and mechanical equipment should be included within buildings or screened from public view in a manner compatible with the overall building design and streetscape treatment of the development.

Guadalupe River

16. **Habitat Preservation.** The riparian habitat of the Guadalupe River and adjacent areas west of Lelong Avenue should be preserved as open space resources consistent with the Riparian Corridor Policy Study.
17. **Bypass Channel Design.** An open channel design for the flood control by-pass channel adjacent to the Elks Club site is the preferred design solution and should be implemented if the site is redeveloped. The culvert design solution should be avoided, if feasible, even if the site is not redeveloped in the near future. (The current plan proposed by the Santa Clara Valley Water District is a bypass concrete box culvert.)

Open Space and Landscaping

18. **Landscape Plans.** Landscape plans and landscaping for public and private development should conform to the City of San Jose Landscape and Irrigation Guidelines. Tree species used in landscaping should be similar to those found in Willow Glen, and along the Guadalupe River.
19. **Residential Open Space.** A system of attractive, on-site open spaces, common areas, and other amenities should be integrated into all new residential development to create comfortable areas for people to meet, interact, and enjoy open space amenities.

ALMADEN ROAD SUBAREA

Subarea Boundaries. The Almaden Road subarea is generally bounded by West Alma Avenue on the north, Little Orchard Street on the east, the Union Pacific Railroad on the south, and Southern Pacific Railroad to the west. The subarea is bisected by Almaden Road which runs in a north-south direction and contains six travel lanes that divide the eastern and western portions of the subarea.

Residential Development. There are two major opportunities for high density residential development or redevelopment in the subarea. The Italian Gardens site (about 8.9 acres) which is located on the east side of Almaden Road and the combined Smith/Cristina properties (about 10.2 acres) which are located on the west side of Almaden Road.

The Italian Gardens site currently accommodates a large banquet/entertainment facility and about four acres of vacant land. The Smith/Cristina properties contain light industrial uses and a small amount of commercial uses. Although the Tamien Station Area Specific Plan would allow these uses to continue indefinitely, the Plan also recognizes that both of these sites could provide housing opportunities similar to existing high density residential development in the area. Since no public open space is located within the subarea, new residential development must provide a substantial amount of open space to serve the new residents that will occupy these projects.

Any new residential development in the eastern portion of the subarea must be designed to minimize the exposure of residents to the potential adverse impacts of existing, adjacent industrial development. New residential development on sites previously occupied by industrial uses will have to take appropriate measures to substantially eliminate any contamination by hazardous materials.

Commercial Development. Although the primary thrust of the Tamien Station Area Specific Plan is to encourage more high density residential uses close to transit, the new population generated by these uses as well as existing residents will need some new support commercial uses. In the Almaden Road subarea, a small cluster of commercial uses is proposed around the intersection of Almaden Road and Alma Avenue. The commercial uses to be located in this area should be small retail shops, restaurants or service uses designed primarily to serve local residents. These uses should also serve potential customers generated by traffic on Alma Avenue and Almaden Road.

Industrial Development. No new industrial development is anticipated in the Almaden Road subarea but existing industrial uses will be allowed to remain indefinitely. In the long term, it is expected that the industrial uses on the Smith/Cristina properties will be replaced with high density residential use. Stucco Supply is expected to remain in its present location for the foreseeable future. Should the owner of the Stucco Supply site, however, wish to terminate the industrial use in the future, the

City should consider a change to a residential use consistent with adjacent residential uses.

LAND USE POLICIES

1. **Housing Variety.** A range of high density housing types suitable for a variety of household sizes is encouraged. A mix of rental and ownership housing is also encouraged.
2. **Residential Density.** New residential development on land designated Very High Density Residential (25-40 DU/AC) is strongly encouraged to achieve an average density of 30 dwelling units per net acre.
3. **Conversion of Existing Industrial Uses.** Although existing industrial uses are allowed to continue in the interim, the industrial lands located at the northwest corner of Almaden Road and the Union Pacific Railroad (the Smith/Cristina properties) represent a valuable housing opportunity and should eventually be converted to high density residential use.
4. **Commercial Use.** Commercial development around the intersection of Almaden Road and Alma Avenue should consist of small scale, neighborhood serving retail and service uses. The intensity of commercial development should range between a Floor Area Ratio (FAR) of 0.25 and 0.30, generating

about 8,000 sq. ft. of new commercial space.

5. **Alternative Commercial Use.** The two sites designated Medium High Density Residential (8-16 DU/AC) located just south of the cluster of commercial sites on the south side of Alma Avenue between Almaden Road and Little Orchard Street may be used commercially if: 1) these sites are combined with the commercial parcels fronting on Alma Avenue; and, 2) if these sites are part of a single commercial project oriented toward Alma Avenue. These sites should be used primarily for parking but should not provide vehicular ingress or egress onto Little Orchard Street.
6. **Italian Gardens.** The existing banquet facility or a new, reduced and/or relocated banquet facility may be incorporated into a new residential project on the Italian Gardens site in the context of a master plan for the site.

DESIGN POLICIES

As is the case with the design policies of the other subareas, a key purpose of these policies is to ensure compatibility with existing neighborhoods. These design policies are consistent with achieving the average densities envisioned in this Plan.

General

1. **Residential Design Guidelines.** New residential development should

conform with the Residential Design Guidelines of the City of San Jose unless otherwise stipulated in the following policies.

2. **Commercial Design Guidelines.** New commercial development should conform with the Commercial Design Guidelines of the City of San Jose unless otherwise stipulated in the following policies.
3. **Security.** Residential development should be designed to enhance the security of residents by not creating hiding places, by providing adequate light (both on-site and off-site), and by creating space that is actively used and observable.

Building Orientation and Design

4. **Building Orientation to Street.** Residential development on the Italian Gardens and Smith/Cristina properties should create a strong "street presence" toward Almaden Road and any public streets to be created on these sites. Buildings should have front entries oriented towards Almaden Road. Buildings should avoid presenting rear elevations, parking or parking structures to Almaden Road.
5. **Architectural Treatment.** The architectural treatment of commercial buildings should be varied and articulated to create interest and diversity. Parapets and roofs should be designed with decorative elements to provide

additional articulation and interest.

6. **Residential Design.** To help larger structures maintain a human scale typical of smaller buildings and to create pedestrian interest, architectural treatments of building facades and roofs should include sufficient articulation and variation to create an impression of individual units or clusters of units compatible with the existing neighborhood and an active pedestrian environment. The use of sloping roofs, decorated parapets, varying floor planes, different but related ornamental elements, balconies, porches, bay windows and other architectural elements is encouraged to achieve variety and articulation.

Building Scale and Height

7. **Residential Scale.** The scale of new residential development in the subarea should be generally consistent and compatible with the existing neighborhood.
8. **Building Height - General.** Building height for residential or commercial development should not exceed 45 feet except for new residential development along Little Orchard Street which should maintain a maximum building height of 35 feet.
9. **Building Height - Italian Gardens.** New residential structures on the Italian Gardens site located within 50

ALMADEN ROAD SUBAREA <i>Key Development Standards</i>

Height	
Italian Gardens	45 feet*
Little Orchard Street Residential	35 feet
All Other Residential and Commercial	45 feet
Setbacks	
<i>Residential</i>	
Italian Gardens	50 feet from Industrial land uses.
From Almaden Road	25 feet*
From Union Pacific Railroad	100 feet*
All other setbacks	Residential Design Guidelines
<i>Commercial</i>	
From Alma Avenue	Max. 5 feet
All other setbacks	Commercial Design Guidelines
Parking	
Residential and Non-Residential	San Jose Zoning Ordinance*

* Refer to policies for additional limitations or exceptions.

feet of the rear property line of the single-family lots facing Little Orchard Street should be limited to two stories and no more than 25 feet in height.

Setbacks and Buffers

10. **Almaden Road - Residential.** New residential development along Almaden Road should maintain a setback of 25 feet or larger consistent with the setbacks of adjacent residential development.

11. **Alma Avenue Commercial Setback.** New commercial development along Alma Avenue should be designed to be pedestrian oriented with a maximum five foot front setback and attractive entryways and display areas.

12. **Italian Gardens - Residential.** Residential dwellings on the Italian Gardens site should be buffered by landscaping and soundwalls and setback 50 feet from planned industrial land uses. When possible, parking or driveways should intervene between dwelling units and industrial uses. Landscape areas

should be a minimum of 10 feet wide.

14. **Little Orchard Street - Residential.**

New high density residential development on Little Orchard Street should be designed so that it is adequately buffered from existing industrial use and can act as an effective buffer between those industrial uses and existing lower density residential uses.

14. **Noise and Vibration -**

Smith/Cristina Site. To avoid the adverse impacts associated with noise and vibration, new dwelling units on the Smith/Cristina site should be setback a minimum of 100 feet from the Union Pacific Railroad tracks. This area of the site could be utilized for parking or driveways. These setbacks may be reduced if appropriate noise and vibration reduction measures can be implemented which provide equivalent mitigation.

15. **Commercial Buffering.** All new commercial development should provide adequate buffering and screening using soundwalls and landscaping adjacent to existing or new residential development. Landscape areas should be a minimum of five feet wide.

Parking and Servicing

16. **Commercial Parking Lot**

Screening. Parking for commercial uses should be partially screened from view from the street by buildings, landscaping, and low architectural walls.

17. **Treatment of Residential Parking**

Facilities. Parking facilities should not be generally visible from Almaden Road. Driveway curb cuts and garage entries should be few to avoid detracting from the pedestrian environment. Parking stories located beneath buildings should be depressed and should not rise above grade more than four feet. Porches, stairways, and other architectural treatments should be used to help screen parking lot openings other than those necessary for driveways. Landscaping may be used to supplement the screening provided by architectural treatments. Parking may be located at or above grade if the parking area will be encapsulated by ground-level residential uses. Because blank walls tend to depress pedestrian environments, blank walls more than four feet high should not be used to screen parking areas from the street.

18. **Service Facilities Screening.**

All garbage and refuse facilities and mechanical equipment should be included within buildings or screened from view in a manner

compatible with the overall building design and streetscape treatment of the development.

19. **Parking Standards.** Parking standards for new residential and commercial development in the Almaden Road subarea are the same as those called for in the City of San Jose Zoning Ordinance.

Open Space and Landscaping

20. **Existing Trees - Italian Gardens.** The mature trees clustered around the existing buildings on the Italian Gardens site should be preserved. These trees should also be integrated into any future residential development of the site or any future modifications to the banquet facility.
21. **Residential Landscaping.** Landscaped areas, a minimum of 10 feet wide, should be provided around the perimeter of all residential projects.
22. **Residential Open Space.** A system of attractive, on-site open spaces, common areas, and other amenities should be integrated into all new residential development to create comfortable areas for people to meet, interact, and enjoy open space amenities.
23. **Triangular Parcel.** The remnant right-of-way parcel shaped like a triangle and located at the northwest corner of Alma and Almaden Avenues, should be landscaped and, if possible, contain a sculptural or

other artistic element that will create a gateway or sense of entry for the Tamien Station area. The City should consult with area residents and, perhaps, the Arts Commission to identify an appropriate theme for these improvements and explore ways these improvements could be funded.

24. **Landscape Plans.** Landscape plans and landscaping for public and private development should conform to the City of San Jose Landscape and Irrigation Guidelines. Tree species used in landscaped areas should be similar to those found in adjacent neighborhoods.

STREET AND CIRCULATION SYSTEM

The existing street and circulation system comprises the framework for the Tamien Station area. It consists of a grid system of minor public streets and sidewalks, several major arterials, and State Route 87 and its bicycle facility. In addition to accommodating vehicular and train movements, the existing system allows pedestrian access from neighborhoods to multi-modal transit facilities, and the Willow Street Neighborhood Business District and other pedestrian shopping areas. The existing street system is expected to remain intact (see Figure 9). No new streets are required, or precluded, by the specific plan.

One of the primary purposes of the specific plan is to create a pedestrian friendly environment by enhancing the existing system of sidewalks with the addition of street trees, street lighting, and street furniture and by building sidewalks where none currently exist. The specific plan also intends to encourage additional walkways and connections, where necessary, to enhance the existing pedestrian and bicycle network (see Figures 11 and 12). Another strategy of the specific plan is to use public sidewalks to connect pedestrians to public spaces, such as the public park near the CalTrain Station or new shopping areas where they can interact with others.

STREET AND CIRCULATION POLICIES

Streets

1. **Key Arterials.** Almaden Road and Alma Avenue should continue to function as key arterial streets in the regional traffic circulation system.
2. **On-Street Parking.** Except on Alma Avenue and Almaden Road, on-street parking should be encouraged to minimize demand for surface parking lots, and to create a buffer for pedestrians using adjacent sidewalks.
3. **Freeway Access and Egress.** The operation of Lelong Avenue as the key access and egress to and from State Route 87 should be preserved and maintained. New development in the Tamien Station area should not significantly interfere with the operation of Lelong Avenue.
4. **New Streets.** If new public streets are required to provide access to new development, these streets should comply with the City of San Jose's street standards.
5. **Vehicular.** Vehicular access to Almaden Road from the Italian Gardens and Smith/Cristina sites should be limited to avoid potential traffic safety hazards.

Pedestrian Walkways

6. **Pedestrian System.** A system of improved walkways, consistent with the circulation policies of this plan, should be created to improve linkages between new residential development, transit facilities, and pedestrian shopping.
7. **Sidewalks and Streetlights.** Existing sidewalks and street lights should be repaired and new sidewalks and street lights added in the Tamien Station area to create a fully functional pedestrian network that will allow residents both within and near to the specific plan to walk safely and quickly to the multi-modal transit facilities and to satisfy day-to-day shopping needs without using an automobile.
8. **Connections to Willow Street Neighborhood Business District.** Improved pedestrian facilities, including enhanced and/or additional street lighting, should be provided along Lick Avenue and Lelong Street to better connect the Tamien Station area to the Willow Street Neighborhood Business District.
9. **Lick Avenue Pedestrian Improvements.** Lick Avenue should be designed and improved with sidewalks of sufficient width to accommodate pedestrians, on-street parking as a buffer from traffic for pedestrians, and a pleasant pedestrian environment with street trees, landscaping, and street furniture.
10. **Pedestrian Improvements - New Development.** Detached sidewalks and planting strips with street trees should be constructed along streets adjoining new development. In high traffic or commercial areas, tree wells may be used in lieu of planting strips.
11. **Street Trees.** Street trees should be planted on all streets within the specific plan area, particularly adjacent to new development, at intervals of 30 feet on center to:
 - Create an unified image of the streets based on a street tree theme for the neighborhood as approved by the City Arborist;
 - Grow an effective canopy along the streets to create shady walkways;
 - Fill in gaps where rows of existing street trees are incomplete; and,
 - Conserve energy and water.
12. **Almaden Road and Alma Avenue Improvements.** For the short term, sidewalk improvements, street furniture, and street trees and other landscaping should be installed along both sides of Almaden Road and Alma Avenue to create "pedestrian friendly" linkages to the transit facilities and the existing neighborhood. The existing set of handrails below the SPRR bridge over Alma Avenue should be

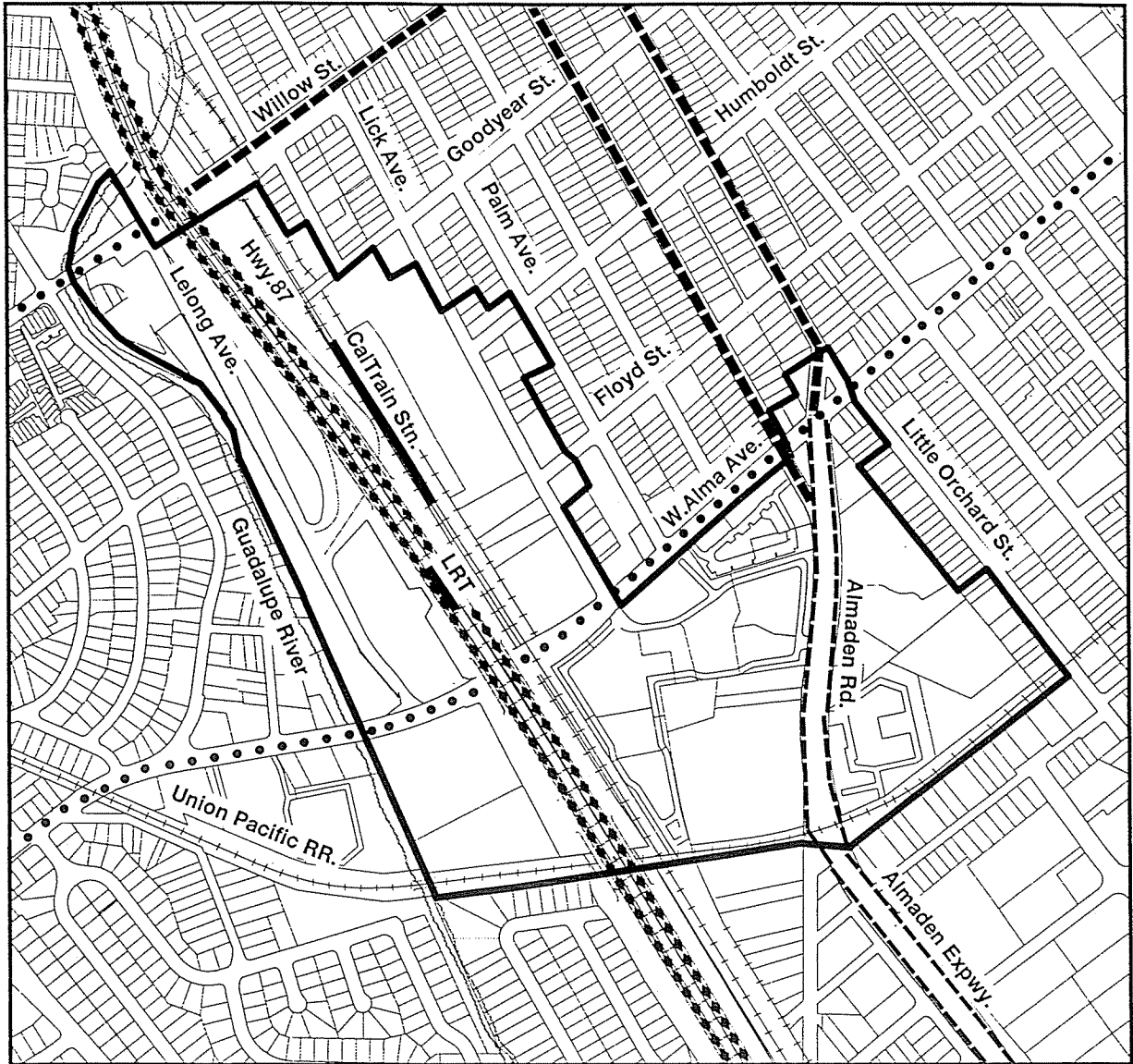
repaired or replaced. New handrails should be extended below the Route 87 freeway bridge to a distance of 50 feet west of the freeway bridge. New handrail design should be compatible with the existing handrail.





13. **Pedestrian Crossing of Almaden Road.** For the long term, the City should study feasible options for creating a pedestrian crossing of Almaden Road between the Italian Gardens and Smith/Cristina sites. These options may include an elevated pedestrian crossing or an at grade crossing controlled by a traffic signal. Initially, the pedestrian crossing will link residential development east of Almaden Road to improved pedestrian facilities on the west side of Almaden Road. In addition, a pedestrian link between the Smith/Cristina site and Lick Avenue through the existing Shadow Run project should be explored. The new pedestrian route created by these links would make it easier and safer for residents in the new residential developments to walk to the transit facilities.

with the State Route 87 bicycle facility and the network of trails along the Guadalupe River by clearly identifying points of access to the State Route 87 bicycle facility from public streets or pathways. New development should not impair this access and should provide connections that allow new residents to access bicycle facilities.

Bicycle Circulation

14. **Street Improvements for Bicycles.** The existing street system should be improved to enable bicyclists to access the multi-modal transit station by clearly designating bicycle lanes and identifying approved bicycle routes with signage.
15. **Bicycle Linkages.** The existing street system should form linkages



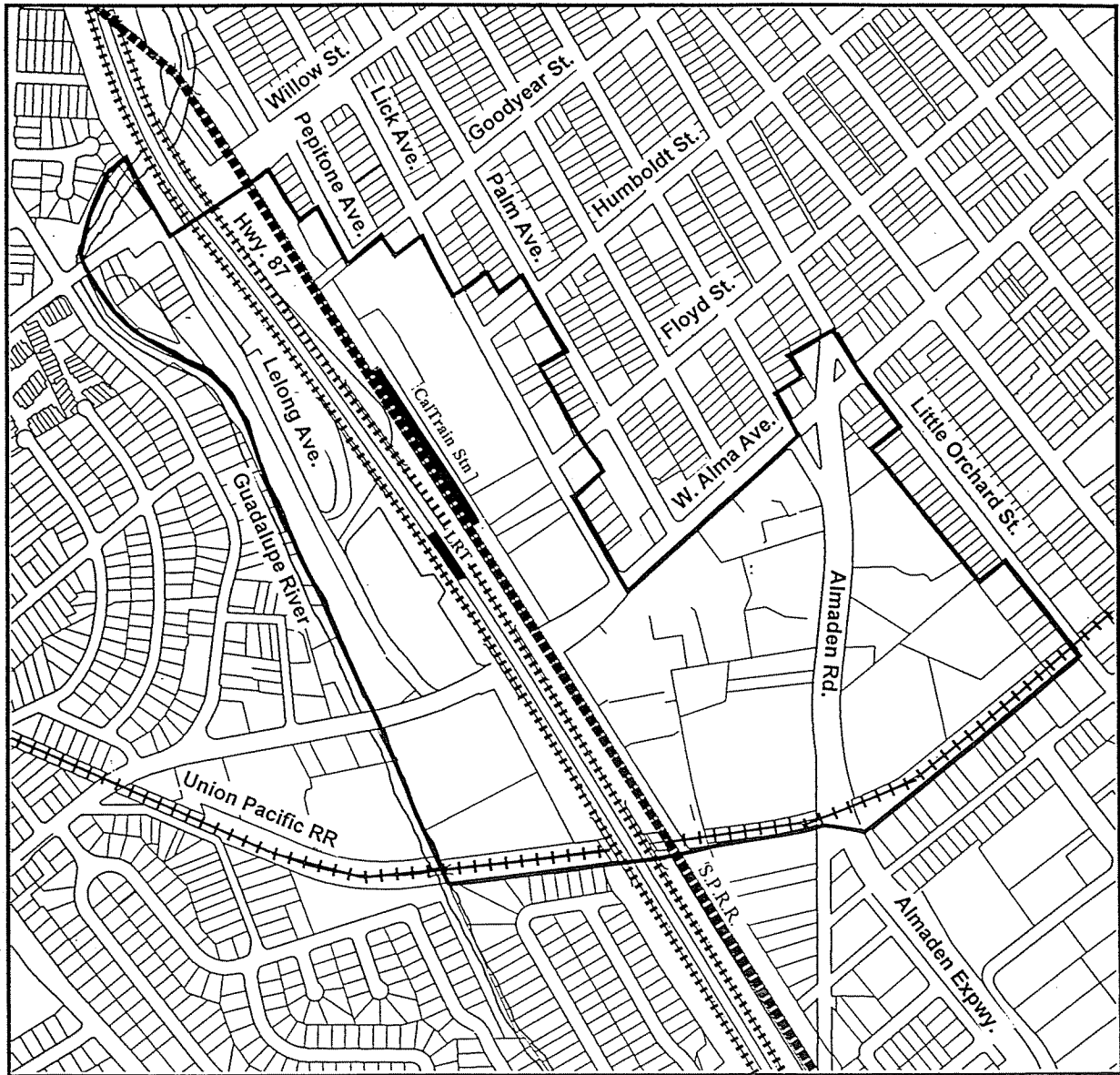
-  State Transportation Corridor (includes freeways)
-  Expressways
-  Major Collector
-  Arterial

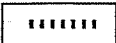


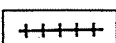


Scale: 1"=750'

RAIL FACILITIES

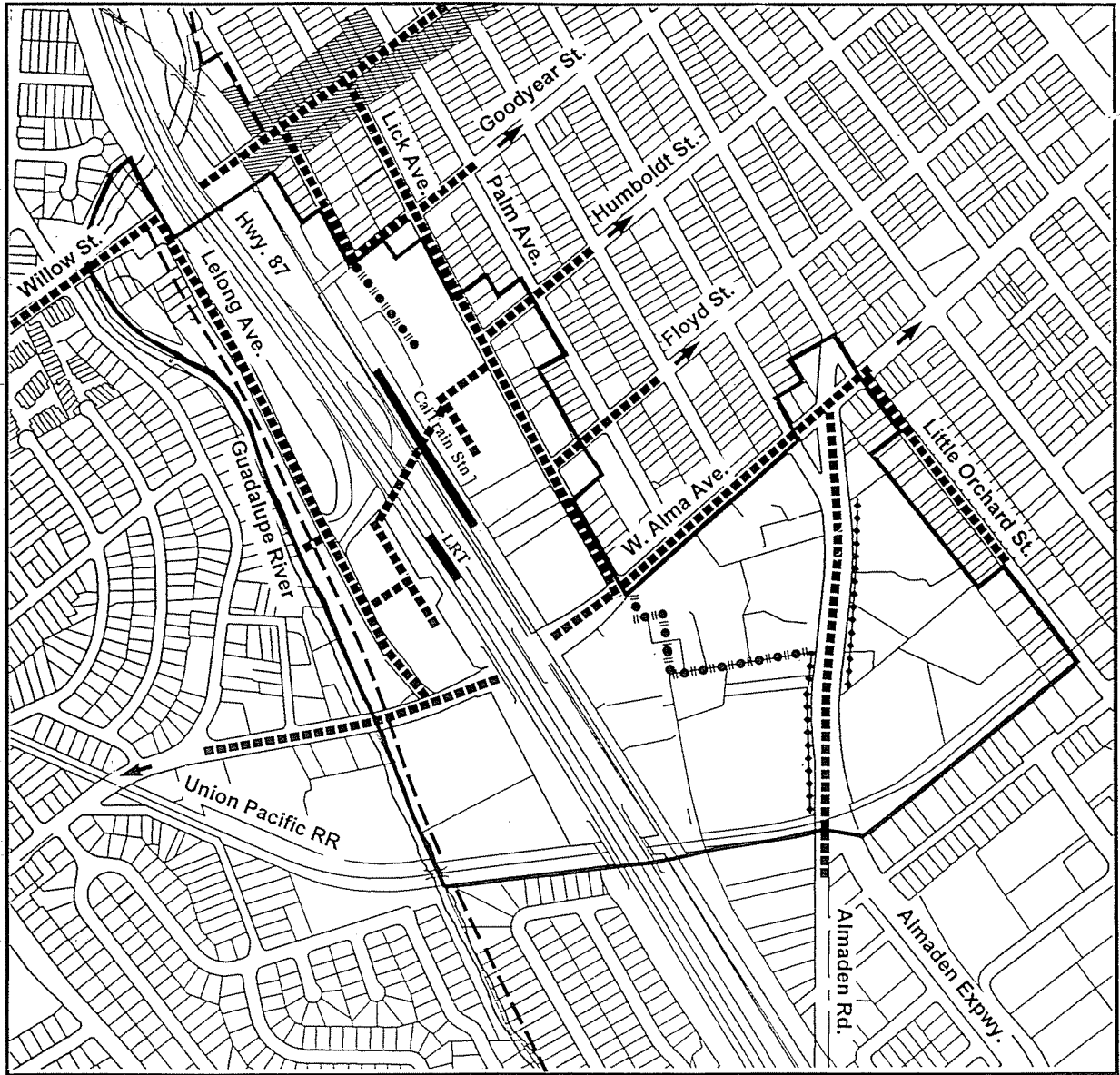
Figure 10

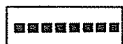
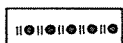



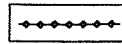

-  Light Rail Transit
-  LRT and Cal Train Stations
-  Cal Train/ Passenger Rail /S.P.R.R.
-  Union Pacific RR/ Freight Rail



Scale: 1"=750'



-  Pedestrian Route - Existing
-  Pedestrian Route - Proposed
-  Pedestrian Trail - Planned

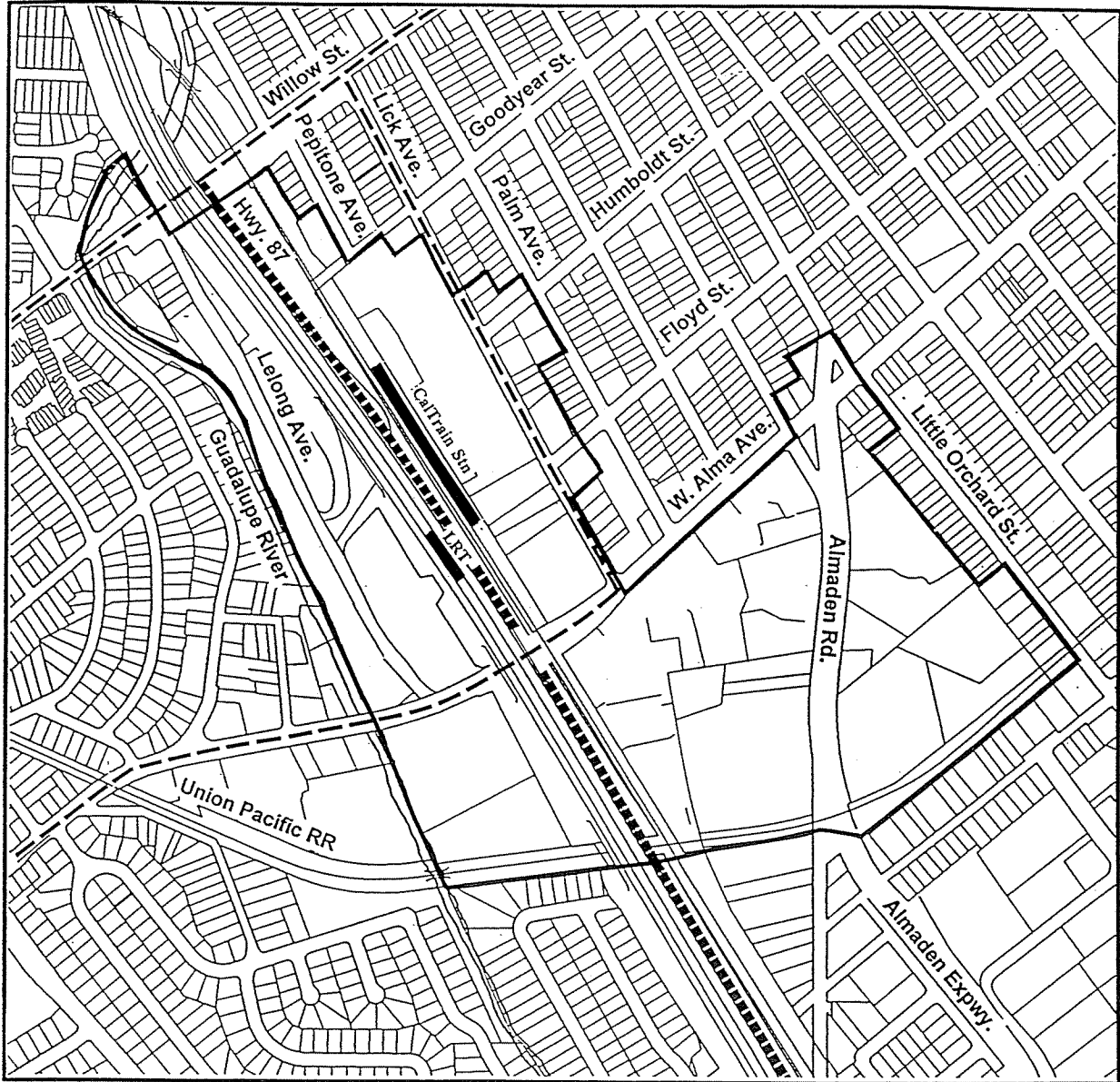
-  Incomplete Sidewalk
-  Willow Street NBD

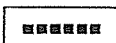
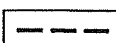


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BIKE FACILITIES

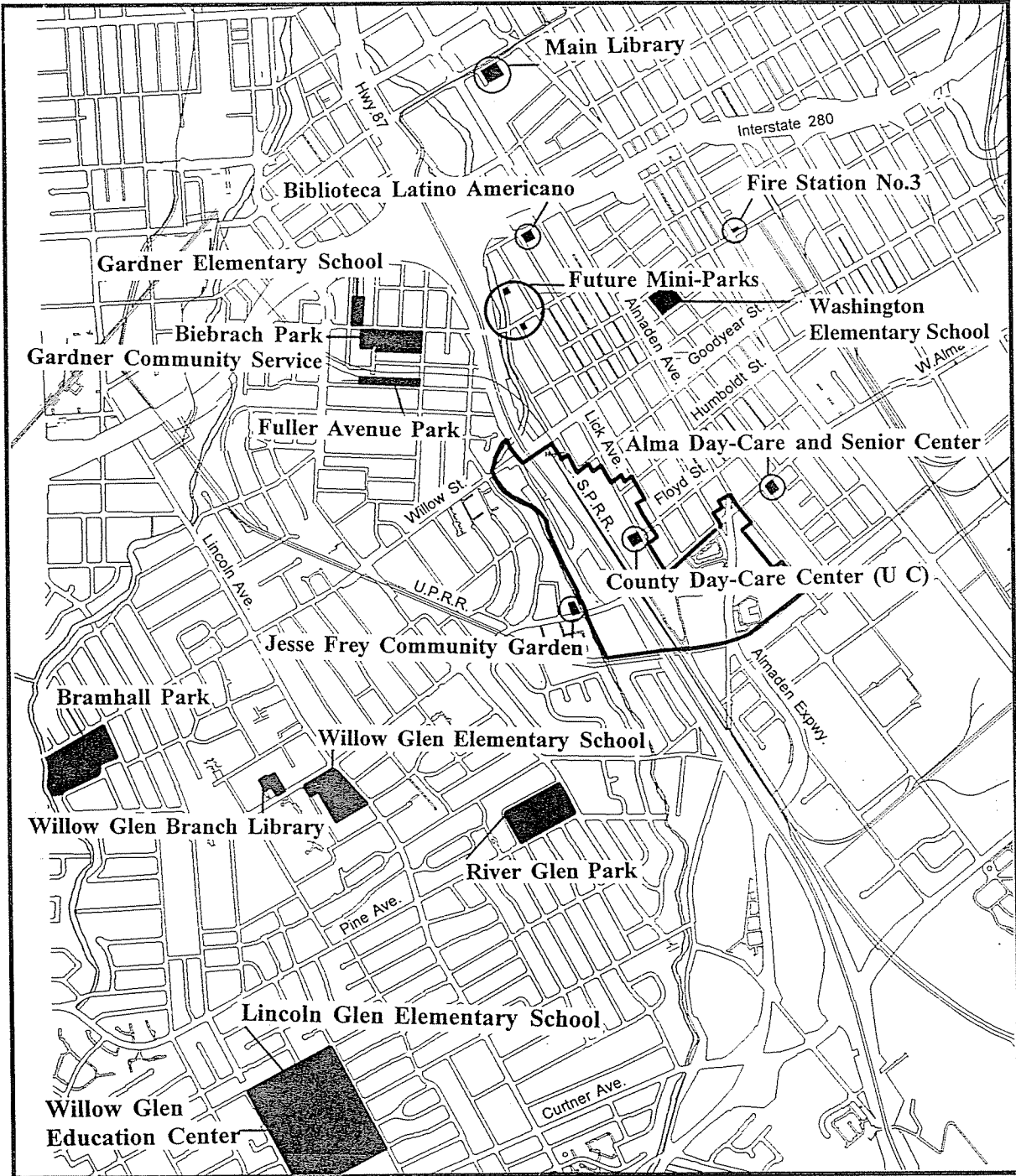
Figure 12



-  Bike Facility - Existing
-  Bike Facility - Proposed



Scale: 1"=750'



Scale: 1"=2700'

COMMUNITY FACILITIES, SERVICES AND UTILITIES

General

The purpose of this chapter is to describe, in general, the community facilities, services, and utilities that are or will be available to residents and businesses in the Tamien Station area. There are no preexisting, publicly owned community service facilities located within the Plan area other than the Guadalupe River right-of-way and the rail facilities around which this plan is structured. The Plan includes a four acre park, a community garden, a child day care center, and a new flood protection facility and river trail, but other community services, such as schools, libraries and larger parks, are located outside the Plan area and will be shared with other neighborhoods (see Figure 13).

Parks and Open Space

The closest parks are located between 0.5 and 1.25 miles from the Tamien Station area and include Biebrach, River Glen, and Bramhall (Willow Glen) parks. Two small parks are also proposed nearby. Although these parks provide some recreational opportunities for the Tamien Station area, more park land will be necessary to serve both existing and new residents.

The Tamien Station Area Specific Plan includes a four acre park near the CalTrain Station entrance west of Lick Avenue. This will provide centrally located public open space suitable for a variety of recreational or

public activities. The park could act as a plaza or gathering place for area residents during community events and as a "grand" entry highlighting the Tamien CalTrain Station. This park will not be sufficient to meet all the recreational needs of the areas residents based on the General Plan policy used to measure park needs. There is not sufficient available land, however, to meet these needs. To partially offset this lack, the Plan proposes to make the most of the 10 acres of open space associated with the Guadalupe River bypass channel. This open space, however, is not suited to meet the need for active recreational space and can only be considered supplemental. A trail is proposed along the bypass channel and will link Tamien residents to other open space and recreational areas. Small open space pockets or rest areas may be provided along this trail.

The Plan also calls for a small community garden on about 0.89 acres located on the east side of Lelong Avenue and north of the small LRT parking lot when sufficient demand is generated due to population growth and sufficient funding is available to make a community garden practical. This parcel is a remnant piece of the Route 87 right-of-way and its proposed use as a community garden reflects the plan's efforts to make the most of every open space opportunity. In addition to this proposed community garden, the existing Jesse Frey Community Garden, located on the north side of Alma Avenue just west of the specific plan boundary, provides another open space resource close to the Tamien Station area.

The General Plan establishes a benchmark of 3.5 acres of park per 1,000 new residents. Considering the population expected at this time from the buildout of the specific plan, there will be a need for approximately 10 acres of park land. Because the specific plan identifies four acres of park land, it will be important to identify additional land for park development. As the specific plan develops, the City should work with developers to attempt to achieve the benchmark for park acreage provided in the General Plan.

Schools

The Tamien Station area is located within the San Jose Unified School District. No schools are located in the Tamien Station area. The nearest elementary schools are (1) Willow Glen Elementary School located approximately one mile from the Tamien Station area at the southwest corner of Minnesota Avenue and Lincoln Avenue, (2) Gardner Elementary School located about 0.66 miles from Tamien on the east side of Illinois Street, north of West Virginia Street, and (3) Washington Elementary School located about 0.51 miles from the Tamien area at the northeast corner of Edwards Avenue and Sherman Street. The nearest middle and high school is the Willow Glen Educational Park Center (North and South) located on Cottle Avenue between Dry Creek Road and Curtner Avenue, approximately 1.5 miles from the Tamien Station area. New elementary school and middle school facilities will be needed to accommodate anticipated growth in the District including in the Tamien area.

To assist the school districts in San Jose in their efforts to provide school facilities for students generated by new residential

development, the City has adopted a School Facility Availability Ordinance. The ordinance specifies that the Council must find that school facilities would not be unreasonably burdened by a residential project prior to approval of that project. There are two conditions under which the Council could find that a residential project will not unreasonably burden a school facility:

1. Evidence is submitted showing that there is school capacity available for the project.
2. The project applicant pays, or offers to pay, a specified fee per square foot of residential development for the purpose of school facility construction.

The Council could also find that the project's benefits outweigh its burden on school facility availability and approve the project. If the project does not have these benefits, however, one of the two conditions listed above must be met. This ordinance, therefore, provides an important additional tool for the school district in its efforts to provide school facilities.

Child Day Care

The Santa Clara County Transportation Agency is constructing a child day care center within the parking lot for the Tamien CalTrain Station in the Transit Core Subarea. The project is located on the west side of Lick Avenue opposite Floyd Avenue. The 150-child center will eliminate approximately 132 of the 427 (31%) existing parking spaces in the CalTrain station park-and-ride lot. About 30 existing parking

spaces will be reserved for the project.

The project includes a one-story building of about 9,000 square feet, and an adjoining play yard of about 20,000 square feet. The child day care center would be operated by either a private or non-profit child day care provider and a staff of approximately 20 persons. The Santa Clara County Transportation Agency is using both Federal and local funds for the project. The project broke ground in October 1994 and is expected to be completed in 1995.

Other Community Services

The Tamien Station area is within San Jose's Urban Service Area and can be adequately provided with services such as police and fire protection. The same is true of library and solid waste services. Tamien is located near the center of the City allowing residents to have relatively easy access to most community services. These services may be strained over time, however, as the City's population grows.

Utilities

One of the advantages of the Tamien Station area is that it is located near the center of the City's Urban Service Area and is well provided with urban services. These services include sanitary sewer/wastewater treatment, water, storm drainage, electricity and natural gas, and communications. The level of development proposed in the Tamien Station Area Specific Plan will place increased demands on these services and will require some service infrastructure improvement or upgrading.

With minor upgrades, sanitary sewer/wastewater treatment facilities, electricity and natural gas lines, and communications facilities can adequately handle the service demands likely to be generated by new development in the Tamien Station area. Water service and storm drainage will also be adequate with some on-site and off-site improvements, including new or upgraded water and storm drainage lines.

The most critical of these new improvements is the upgrading of water lines to provide necessary fire flow and domestic water service for the increased intensity of development. The details of the necessary water improvements can not be known until specific development proposals are made, however, areas currently served by 4-inch or smaller water lines would require upgrading in order to meet basic fire flow requirements. The water service mitigation implementation policy (see Implementation Chapter) will help ensure that sufficient water will be available for the Tamien Station area.

There is sufficient storm drainage capacity to serve anticipated development in the Transit Core Subarea. New storm drainage facilities in this subarea would connect to existing County Transit District and/or CalTrans storm drainage facilities.

In the Guadalupe River Subarea, the Santa Clara Valley Water District flood control bypass channel will be the most significant "storm drainage" facility which, of course, serves a much larger geographic area than Tamien. Even with these improvements, however, the Elks Club site may still be

subject to on-site flooding. This situation could affect the ability of new development on the Elks Club site to use depressed parking unless substantial flood proofing improvements, such as ramps and mounds, and/or floodgates, were included in the project. Another solution would be to raise the elevation of the site so that, in the event of on-site flooding, storm waters could release to the existing storm drainage facilities on the northerly side of Alma Avenue. The new on-site storm drainage system could either outfall to the Guadalupe River, connect to the existing 54-inch line along the southern boundary of the site, or connect to the County Transit storm drainage facilities on the northern side of Alma Avenue.

New development within the Almaden Road Subarea would connect to the existing 24-inch and 27-inch storm lines west of Almaden Road and to the 18-inch line within the Almaden Road right-of-way. In order to accommodate new development within this subarea, a segment of 15-inch line between the northerly terminus of Almaden Road and the Almaden Avenue/Alma Avenue intersection would require upgrading to at least a 24-inch line.

The storm drainage facilities mitigation and flooding mitigation implementation policies (see Implementation chapter) will help ensure that adequate flood control protection is provided to the Tamien Station area.

IMPLEMENTATION

The implementation chapter provides a framework to assist in carrying out the Tamien Station Area Specific Plan. The framework contains four major components as follows:

- Land Use Regulation Policies
- Incorporation into, and Consistency with, the General Plan
- Consistency with Other City Policies and Programs
- Administration of the Tamien Station Area Specific Plan

LAND USE REGULATION

The Tamien Station Area Specific Plan is a long-term plan providing direction for development and redevelopment of this area. The Tamien Specific Plan is incorporated into the San Jose 2020 General Plan and, therefore, covers the same timeframe as the General Plan. The purpose of this section is to provide the policy guidance necessary to implement the Tamien Station Area Specific Plan over the entirety of this timeframe.

Existing Land Uses. The specific plan allows existing land uses in the Tamien Station area to remain indefinitely recognizing that plan implementation will gradually occur over a period of years. As these existing uses age or as their economic value is reduced, it is expected that they will be replaced by the uses designated in the land use plan. To encourage this transition from existing to planned land uses, the implementation policies seek to limit the remodeling or expansion of existing industrial or commercial buildings to a reasonable level consistent with maintaining the viability of existing uses.

Interface Issues. The implementation policies are designed to help ensure the achievement of the major goals of the specific plan which include the creation of a transit oriented, high density residential and mixed use community that is "pedestrian friendly" and properly integrated with existing neighborhoods. One of the key challenges for implementation of the specific plan will be to develop compatible interfaces between new residential or mixed use development and existing residential and industrial uses. Some of the potential interface problems will be temporary, such as the interface between new residential uses and existing Lick Avenue industrial uses which will eventually be replaced by high density residential uses. Regardless of the timeframe, industrial and residential land use interfaces should be treated with care to avoid potential adverse impacts and neighborhood complaints. The interface between existing lower density residential neighborhoods and newer high density residential development must also be treated carefully to maintain existing neighborhood character, including single-family character, while developing strong connections between existing and new parts of the neighborhoods.

Master Planning. To ensure that the Tamien Station Area Specific Plan is efficiently and effectively implemented, the Plan calls for the master planning of certain key sites and property clusters. Master planning of these sites will provide for more detailed analysis and create more detailed development plans to achieve the objectives of the Plan. For those sites that are a cluster of parcels, master planning is intended to help ensure that "piecemeal" development of individual parcels will not impair the full

development of other parcels in the development clusters identified in the Plan. The master planning process can also be used to resolve important issues, such as a more precise location for the four acre park on the SCCTA site, which cannot be fully addressed in the Plan itself. In general, each master plan area should be the subject of a single Planned Development zoning district to help ensure that future development accurately follows the master plan.

Master plans may be developed in conjunction with Planned Development zonings or as separate documents which should be reflected in subsequent Planned Development zonings. Master plans should address the specific location of uses and buildings, establish pedestrian and vehicular circulation patterns, provide for open space and other amenities, identify and provide for infrastructure needs, and establish compatible architectural styles and landscaping. Master plans should be the result of joint planning between affected property owners who should work together to reach agreement on the form and timing of development. Subsequent development approvals should be consistent with the master plan.

Timing. Implementation of the Tamien Station Area Specific Plan will take both perseverance and patience since it may take 10 or more years to accomplish all the changes envisioned in the plan. The following policies are intended to help keep the plan "on track" and to smoothly guide the transition from existing to planned uses. These policies should be used in conjunction with the goals and objectives and other policies of the Tamien Station Area Specific Plan.

Implementation Policies

1. **Common Improvements and Amenities.** When development is proposed in the specific plan area, City staff should identify all those off-site improvements and amenities called for in the plan which are proximate to the development site and/or would serve the future residents and users of the site, and should, with the applicant, consider how best they might be provided. The City may consider applying all parkland dedication and other applicable fees generated in the Tamien Station area to the costs of developing the public facilities and improvements identified in this Plan.
2. **Planned Development Zoning.** High density residential and mixed use development may be pursued through the Planned Development zoning process to provide the flexibility and control necessary to fully implement the Tamien Specific Plan.

Existing Uses/Buildings

3. **Existing Land Uses to Remain.** Existing land uses which do not conform to the Tamien Station Area Specific Plan, or similar land uses allowed in the existing zoning district, may remain indefinitely until a property owner wishes to change uses.

4. **Industrial and Commercial Remodeling or Reconstruction.** Only minor building improvements for existing industrial or commercial buildings or facilities on properties designated for residential or mixed use should be allowed. These improvements should not change the character of existing structures and should not add more than 25% to the pre-improvement value of these structures. Remodeling or reconstruction of buildings should not use upgraded structural system types.
5. **Industrial or Commercial Expansion.** Expansion of existing industrial or commercial buildings on sites designated for residential use or mixed use should be minor and limited to no more than 10% of the existing building floor area.
6. **Landscaping and Off-Site Improvements for Remodeling or Minor Expansions.** The City should minimize its landscaping and off-site improvement requirements commensurate with the level of on-site improvements associated with the remodeling or minor expansion of existing industrial or commercial buildings on properties designated for residential use or mixed use.
7. **Industrial or Commercial Building Remodeling Materials.** Any expansion or remodeling of an industrial or commercial building should use materials which do not require significant investment and that are not substantially higher in quality than existing materials.
8. **Industrial/Residential Interface.** The interfaces between industrial and residential uses should be carefully treated to protect existing businesses and future development opportunities on adjacent properties. The interface between new residential development and existing industrial sites planned for residential development should address the potential land use conflicts between existing industrial uses and new residential uses, but should also avoid creating negative conditions for future residential redevelopment of adjacent industrial sites.
9. **Residential/Non-Residential Land Use Conflict Mitigation.** New residential development should mitigate potential land use conflicts with existing industrial and commercial uses by locating driveways and parking areas adjacent to these uses to create effective on-site buffers.
10. **Industrial/Commercial Mitigation.** Any industrial or commercial remodeling or expansion or any new commercial development should provide adequate mitigation for nearby existing or planned residential uses by locating noisy activities or operations away from residential property lines or by providing effective soundwalls and landscape screening. Industrial and commercial uses on the Smith/Cristina site are exempt from this mitigation provision in terms of *Mitigations/Interfaces*

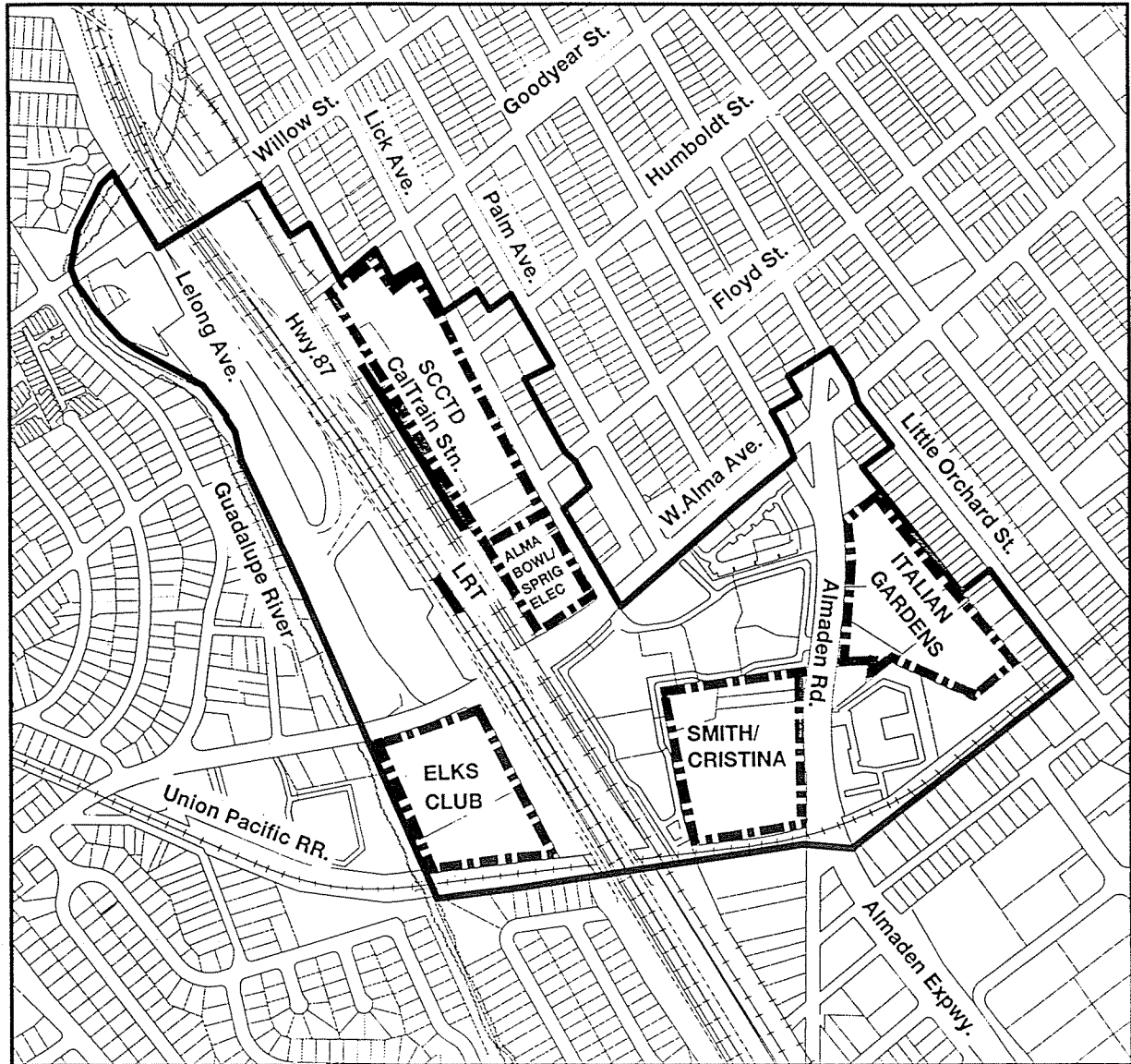
the residential uses planned for that site since any necessary mitigation measures will be incorporated into the master planning for the future residential use of that site.

11. **Hazardous Materials Mitigation.** A soil and/or groundwater analysis should be prepared prior to new development in areas where there has been prior hazardous materials use or storage to determine the extent of contamination and to identify necessary mitigation measures. Hazardous materials or contaminants should be satisfactorily eliminated before allowing the conversion of these sites to industrial uses.
12. **Flooding Mitigation.** If new development in the Specific Plan area proceeds in advance of the implementation of the Guadalupe River Flood Control Project, the new structures within the 100-year flood plain should be constructed at elevations above the 100-year flood level. Property owners proposing new developments in the flood control improvement area may be required to dedicate land or grant easements to the Santa Clara Valley Water District for future flood control facilities.
13. **Future Traffic Mitigation - Lelong Avenue.** At the time new development is proposed that could significantly impact the intersection of Lelong Street and the Route 87 on-ramp, the City should require the project proponent to engage a traffic consultant to conduct a traffic study to determine if the intersection


requires a traffic signal or other improvements to mitigate traffic impacts. If a traffic signal or other mitigation is warranted, new development should provide its share of the required improvements.

14. **Future Traffic Mitigation - Lick Avenue.** Before major new development (100 dwelling units or more) is approved on Lick Avenue, and at the request of neighborhood residents, the City should conduct a neighborhood traffic study consistent with City Council Policy 5-5 (Neighborhood Traffic Management Programs) to determine what adverse traffic effects, if any, might occur on Lick Avenue and other nearby streets. If adverse traffic effects are identified, the study should develop appropriate measures to mitigate these effects such as tree pop outs, "neckdowns", diverters, etc. that are attractively designed. The project proponent and the City should work with the community in the design of the traffic study and in developing and evaluating potential traffic control measures. The City and community should explore ways to finance and maintain these improvements including grant applications, contributions from developers, neighborhood volunteerism, etc.
15. **Noise Mitigation.** A site specific noise and vibration analysis should be prepared for any future residential or mixed use development which is proposed along the Southern Pacific or Union Pacific Railroads and a site specific noise analysis should be

- prepared for any future residential or mixed use development in the vicinity of special noise generators (e.g. industrial uses).
16. **Archaeological Resources Mitigation.** New development on larger parcels should be required to conduct mechanical archaeological testing and, if necessary, more detailed in-field hand excavation and archaeological monitoring if resources are discovered.
17. **Historic Preservation Mitigation.** The Italian Gardens complex should be added to the City's Historic Resources Inventory. In addition, the Historic Landmarks Commission should consider designating this site as an Historic Landmark.
18. **Italian Gardens - Historic Preservation.** Any modification or removal of Italian Gardens complex buildings should be allowed only after the preparation of a more detailed Historic Resources Report which will expand on the initial analysis prepared for the specific plan EIR. This more detailed analysis should consider the historic value of the complex and identify which portions of the complex should be preserved. Although not all buildings on the Italian Gardens site merit preservation, minimum preservation should include the Lo Curto residence, the gardens and mature trees. The master plan for the Italian Gardens site should incorporate the historic elements to be preserved into the development design which should include the restoration or rehabilitation of the structures for adaptive reuse without compromising their architectural integrity.
19. **Water Service Mitigation.** New developments on parcels which are currently served by water lines of 4 inches in diameter or smaller should upgrade water lines to meet basic fire flow and domestic service for new development.
20. **Storm Drainage Facilities Mitigation.** New development should upgrade storm drains in Almaden Road and at the Elks Club site to current City standards. A segment of the 15 inch line between the northerly terminus of Almaden Road and Almaden Avenue/ Alma Avenue intersection should be upgraded to at least a 24-inch line.
- Master Planning*
21. **Santa Clara County Transportation Agency Site (SCCTA).** The City and the Santa Clara County Transportation Agency should cooperate in the preparation of a master plan for the entire 9.5 acre SCCTA site to ensure that the high density residential, public park, parking, and other uses shown on the Tamien Land Use Plan are properly combined and located to effectively support transit use and achieve the other goals of the Tamien Station Area Specific Plan. It would also be advantageous for this site to be master planned in conjunction with the Alma Bowl/Sprig Electric site.



Scale: 1"=750'

 Major Sites in Tamien

22. **East Side of Lick Avenue.** The parcels located on the east side of Lick Avenue are generally small and should be combined in suitable clusters to allow for efficient planning and development. For example, the three parcels located north of Humboldt Street should be combined into a single site and planned as a single project.
23. **Alma Bowl/Sprig Electric.** The Alma Bowl and Sprig Electric parcels located at the northwest corner of Alma Avenue and Lick Avenue should be combined and planned for joint development to allow for efficient use of the site. It would also be advantageous for this site to be master planned in conjunction with the SCCTA site.
24. **Italian Gardens.** New residential development on the Italian Gardens site should be preceded by and consistent with a master site plan and zoning covering the entire site. This master plan could be implemented in phases so that the vacant northern portion of the site could be developed in residential use while allowing the banquet facilities to continue operation or to allow those facilities to be reduced and relocated consistent with the intent of the objectives of this Plan. The first phase of development should be designed to buffer residents from any impacts associated with the operation of the banquet facility.
25. **Elks Club.** New development on the Elks Club site should be preceded by and consistent with a master site plan and zoning covering the entire site. This master plan could be implemented in phases and could include a combination of uses as described in the land use policies of this plan. These uses may include a reduced and relocated freestanding lodge facility, either by itself or in combination with commercial uses, consistent with the intent of the objectives of this Plan. Development should be designed to buffer residents from any impacts associated with the operation of the lodge facility or commercial uses.
26. **Smith/Cristina Properties.** A master site plan and zoning for the residential development of the Smith/Cristina properties should be prepared prior to any new residential development of those properties. If residential development is to occur in phases, the master plan should address how the new residential development will be buffered from existing industrial uses.
27. **North Side Alma Avenue.** The commercial parcels on the north side of Alma Avenue at Almaden Road should be combined and developed jointly with new commercial uses.
28. **South of Alma Avenue, East of Almaden Road.** Any redevelopment of the commercial properties located on south of Alma Avenue and east of Almaden Road, excluding the small retail complex facing Almaden Road, should combine the smaller commercial lots for joint planning and development. The two-story structure located at the

southwest corner of Alma Avenue and Little Orchard Street should be evaluated for its suitability as part of a larger commercial development of this area and, if suitable, should be considered in the preparation of a master plan for this area.

29. **Parcel Combinations.** For each contiguous area listed under the *Master Planning* section, a single master Planned Development zoning is the preferred process for implementing new projects. Alternatively, for areas in multiple ownership, where one or more owners are not in a position to proceed with a master plan, the development of a single parcel or a smaller cluster of parcels that is part of the larger development cluster may be considered if: it is designed to allow the remaining area to develop in an efficient and practical manner, and a master plan showing how this may be accomplished is prepared by the property owner desiring to independently develop a parcel identified as part of a development cluster.

Special Improvements

30. **Special Improvements.** The City should explore a variety of methods to fund or create those community improvements that cannot be provided by private development requirements including: pedestrian improvements such as sidewalks, street trees, handrails, new access through the existing condominium projects on the south side of Alma Avenue, the pedestrian crossing of

Almaden Road; bicycle improvements connections to existing and planned bike trails; and, public landscaping such as new street trees and the "triangle" at the northeast corner of Almaden Road and Alma Avenue. Methods to fund or create these public improvements could include financing mechanisms, such as bond measures, special assessment districts, or grants from programs such as Community Development Block Grants, San Jose Beautiful, etc.

INCORPORATION INTO, AND CONSISTENCY WITH, THE GENERAL PLAN

Tamien Station Area Planned Community. The Tamien Station Area Specific Plan is consistent with San Jose's General Plan. The General Plan incorporates the land use plan and major development policies of the Tamien Station Area Specific Plan through the Tamien Station Area Planned Community (TSAPC) land use designation. The TSAPC contains the major features and provisions of the Specific Plan including the specific land use map for the Tamien Station area. The Tamien Station Area Specific Plan is a separate policy document providing the background, goals and objectives, and community character for the TSAPC and detailed implementation measures beyond the scope of the General Plan.

The Tamien Station Area Specific Plan differs from the TSAPC in that it contains more detailed land use, urban design, and other provisions intended to implement the TSAPC. These more detailed provisions

should be used when evaluating development proposals in the TSAPC.

General Plan Major Strategies. The Tamien Station Area Specific Plan is consistent with the seven Major Strategies, or central themes, of the General Plan: Economic Development, Growth Management, Downtown Revitalization, Urban Conservation/Preservation, the Greenline, Housing, and Sustainable City. The Major Strategies provide a broad framework that allow consistent interpretation and application of the General Plan's individual goals and policies. The Tamien Station Area Specific Plan furthers the Major Strategies by applying the principal objectives of these strategies to the planning of the Tamien Station area.

The Tamien Station Area Specific Plan advances the Economic Development Major Strategy by maximizing the economic potential of the Tamien Station area's commercial and mixed use lands and by supplying housing for the City's labor pool and better connecting that housing to employment centers to the north. It implements the Growth Management Major Strategy by promoting high density, infill development to ensure the efficient delivery of urban services to future residents.

The Downtown Revitalization Strategy is addressed by providing high density housing near Downtown to help support it as a place for business, entertainment, and cultural activity. The Tamien Station Area Specific Plan furthers the Urban Conservation/Preservation Major Strategy by seeking to respect the integrity of the surrounding neighborhoods, and to promote resident's pride in the quality of their living environments. The Tamien Station Area

Specific Plan implements the Greenline Major Strategy by using infill development to reduce development pressures on open space at the fringe of the urbanized area and to protect watersheds, habitat, and recreational opportunities. The Housing Major Strategy is implemented by striving to provide a variety of housing opportunities for all the economic segments of the community close to jobs and urban services. The Sustainable City Major Strategy is supported by the Tamien Station Area Specific Plan since it will improve energy efficiency by encouraging transit use and a compact form of development, use limited land resources efficiently, and minimize waste.

Goals and Policies. The Tamien Station Area Specific Plan is consistent with and reinforces the goals and policies of the General Plan and, therefore, enhances the internal consistency of the General Plan as a whole. The goals and policies of the Tamien Specific Plan "nest" within the goals and policies of the General Plan but are more narrowly defined to ensure proper implementation of the Tamien Station Area Specific Plan.

CONSISTENCY WITH OTHER CITY POLICIES AND PROGRAMS

The Tamien Station Area Specific Plan is consistent with other City policies and programs including the Comprehensive Housing Affordability Strategy (CHAS), and the Housing Initiative.

Comprehensive Housing Affordability Strategy. The Comprehensive Housing Affordability Strategy (CHAS) is San Jose's plan for providing affordable housing using its own and other resources. The CHAS

describes the housing assistance programs and activities administered by the City's Housing Department to provide affordable housing opportunities, including the rehabilitation and construction, of affordable housing. Approval of the CHAS by the Department of Housing and Urban Development (HUD) is a prerequisite for federal funding of local housing projects. The CHAS includes five elements consisting of: (1) a needs assessment, (2) market and housing inventory conditions, (3) strategies to promote affordable housing, (4) an evaluation of resources to meet needs, and (5) an implementation program. The Tamien Station Area Specific Plan is consistent with the strategies described in the CHAS to promote affordable housing.

The potential for affordable housing in the Tamien Station area was considered and discussed during the course of developing the Plan. It was concluded that the Tamien Station area provided excellent potential for affordable housing, particularly with its proximity to public transportation. The Tamien Station Area Specific Plan is expected to continue to help increase the supply of affordable housing by increasing the supply of high density residential lands and by encouraging development to occur at higher densities. By increasing the overall supply of higher density housing, more affordable housing opportunities can be created since higher density housing can achieve lower production costs per unit. Also, since new residential development in the Tamien Station area will be infill development, infrastructure costs should be relatively lower per unit than at the edge of the City. Reduced urban service costs will help the City maintain or improve its levels of service throughout the City.

Housing Initiative. The Housing Initiative is an innovative program established by the City of San Jose in 1989 to encourage the production of high density housing near public transit and other major transportation facilities. The Housing Initiative Study for the Guadalupe Corridor identified several sites in the Tamien Station area that are suitable for high density residential use and recommended the preparation of a specific plan for the area. The Tamien Station Area Specific Plan follows up on the work of the Housing Initiative by designating lands for high density residential use or mixed use close to both the light rail transit and CalTrain stations.

ADMINISTRATION OF THE TAMIEN STATION AREA SPECIFIC PLAN

This section explains the process for maintaining and amending the Tamien Station Area Specific Plan. Any proposed amendments to the Tamien Station Area Specific Plan may only be considered during the City's General Plan Annual Review process. In general, proposed amendments must be consistent with the goals and objectives of the San Jose 2020 General Plan and the Tamien Station Area Specific Plan. Future land use changes in the Tamien Station area may be beneficial under certain circumstances.

Modifications to the Specific Plan

The Tamien Station Area Specific Plan represents a long-term plan for the Tamien Station area. Occasionally, it may be necessary to modify some of the components of the Specific Plan either to reflect changing conditions or to update existing City goals and policies. Such modification should occur as a General Plan amendment during the City's General Plan

Annual Review process. Any modification of the Tamien Specific Plan should be guided by the following criteria:

- Any modification should further, and be consistent with, the Major Strategies of the General Plan.
- Any modification should be consistent with the goals, objectives, and policies set forth in the General Plan and the Tamien Station Area Specific Plan.
- Any modification on a specific site should be compatible with surrounding land uses.
- Any modification should be consistent with other applicable City policies.

Specific land use amendments in the Tamien Station area that may be considered in the future include the following:

Termination of the Stucco Supply

Industrial Uses. If the industrial uses located on the Stucco Supply site (located at the northwest corner of Little Orchard Street and the Union Pacific Railroad) are terminated or abandoned, designation of the site and other adjacent industrial sites from Light Industrial to a residential land use designation should be considered. Density distributions should be consistent with those of this plan and of surrounding properties. In general, this could mean higher density residential uses for most of the Stucco Supply site consistent with the proposed or existing density of development on the adjacent Italian Gardens and Cinnabar apartment complex sites, and, lower densities near the Little Orchard Street side of the area, compatible with existing residential development.

Intensification of Tamien LRT Parking

Lot. If the intensification of land uses in the Tamien Station area is successful over time, the City may wish to consider allowing high density residential or vertical mixed use development utilizing air rights over the Tamien LRT Station parking lot.

Residential use should be at a density similar to that allowed on other sites adjacent to the Tamien LRT Station.



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